



Planning and Community Development Department

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CONSOLIDATED PERMIT DESIGN REVIEW and PARKING ADJUSTMENT PERMIT DECISION AND PERMIT

DR2019-0003, ADM2019-0017, SEP2019-0020

315 N. Samish Way

Type II Decision

I. SUMMARY OF DECISION

Proposal: Phased redevelopment of the former Aloha Motel site located at 315 N. Samish Way. The full redevelopment of this site is proposed to include commercial uses along the entire Samish Way frontage, structured parking and approximately 160 affordable residential units.

Phase 1, which is the subject of this permit, includes 69 apartments, street front office along Samish Way, structured parking, and street improvements on Otis Street with public street parking. The total square footage of the proposed building is 87,320 square feet with structured parking on two levels, and the site is 26,036 square feet. The applicant is requesting departures from design standards. See attached Exhibits for plans and supporting materials.

The proposal includes an application requesting a parking adjustment to reduce the on-site parking requirement from 95 to 72 spaces for affordable housing and also in lieu of other mitigation such as improvement of public street parking (13 stalls), increased bike parking, shared parking, and location on and near WTA transit routes, in accordance with the Samish Way Urban Village Plan.

The project is located within the Samish Way Urban Village Design Review District. A SEPA determination of non-significance (DNS) was issued for the project.

Owner: Bellingham Housing Authority, Attn: Ken Van Winkle; P.O. Box 9701, Bellingham, WA 98227-9701; Ph: (360) 676-6887.

Applicant: Robert Wright, RMC Architects; 1223 Railroad Ave, Bellingham, WA 98225; Ph: (360) 676-7733.

Location: **315 N. Samish Way**, Assessor's #s 380331 421256 0000 and 425243 0000.

Legally described as: ELDRIDGE-BARTLETT'S ADD TO SEHOME LOTS 10 THRU 16 BLK 26-TOG WI VAC SE 10 FT OTIS ST ABTG-TOG WI VAC NE 10 FT LAUREL ST ABTG LOT 16-TOG WI VAC NW 1/2 ALLEY ABTG LOTS 13 THRU 16-LESS RD. ELDRIDGE-BARTLETT'S ADD TO SEHOME LOTS 17-18-19-THAT PTN OF LOT 20 BLK 26 LY WLY OF PACIFIC HWY-TOG WI VAC SE 1/2 ALLEY ABTG-TOG WI VAC NE 10 FT LAUREL ST TOG WI VAC NW 10 FT PASCO ST ABTG LOT 17.

Sehome Neighborhood, Area 15; Samish Way Urban Village – Commercial Transition 2 (CT2) zoning designation.

Decision: Approved with Conditions.

Date: October 22, 2019

II. PERMIT PLANS AND ATTACHMENTS

This approval includes the following documents, subject to any modifications and conditions contained in **Section IV** of this permit:

Exhibit A Design Review Application, Development Objectives, Project Narrative (updated 7/22/2019)

Exhibit B Parking Reduction Application, Parking Reduction Narrative (updated 7/22/2019)

Exhibit C – Phase 1 project site, building design and landscaping plans (updated 7/22/2019)

Exhibit D – Phases 1 & 2 Master Plan with project site, building design and landscaping plans

Exhibit E – Notice of Incomplete Application & NOIA Response

Exhibit F – Public Comments

Exhibit G – SEPA DNS, includes checklist and supporting documents

III. FINDINGS OF FACT AND CONCLUSIONS OF LAW

Applicable Zoning, Standards, and Surrounding Context

1. The subject site proposed for development ("site") is in the Commercial Transition 2 (CT2) land use area of the Samish Way Urban Village (SWUV), Samish Way Urban Village Design Review District, and Area 15 of the Sehome Neighborhood.
2. The triangular site includes two contiguous parcels totaling approximately 65,905 sq. ft. (1.52 acres).
3. The site's topography decreases from west to east down to N. Samish Way. The site is vacant, formerly occupied by the Aloha Motel, except a small parking lot and landscaping remain.
4. Abutting streets and surrounding land uses:
 - East: **N. Samish Way** (platted 80' primary arterial) fully improved with 5 lanes, curb, gutter, and sidewalk. Properties on the opposite side are zoned Commercial Core and developed with auto oriented eating establishments and commercial retail.
 - Northwest: **Otis Street** (platted 60' residential street minimally improved with asphalt and gravel shoulders) followed by a single family home at the SW end of the opposite block face and apartments mid-block, both zoned Residential Transition 1, and commercial retail and services at the NW end zoned Commercial Transition 1.
 - Southwest: **E. Laurel Street** (platted 60' residential street minimally improved with asphalt and gravel shoulders and perpendicular parking along south half of the site frontage) followed by N to S attached single family homes and commercial office, both zoned Commercial Transition 2, and motel zoned Commercial Core.
 - Southeast: **Abbot/Pasco Streets** is an oddly recessed intersection off N. Samish Way that is minimally improved with asphalt and gravel shoulders.
5. **Phase 1**, which is the subject of this permit, includes 69 affordable apartments, street front office along Samish Way, structured parking on 2 levels, and street improvements on Otis Street with public street parking. Vehicular access to the proposed parking structure in Phase 1 will be from Otis Street and Laurel Street. A courtyard will be located on the lid of the structured parking. (**Exhibit C**).
6. Bellingham Municipal Code (BMC) 20.25.020(D) requires design review approval for a development project requiring a building permit within the Samish Way Urban Village

Design Review District. Development of the site requires the issuance of a building permit. Similarly, design review approval will be required at a time TBD for Phase 2.

7. In accordance with BMC 20.25.020(D)(3)(b), the subject design review decision shall be based on the project's consistency with the Urban Village Design Standards (UVDS) in Chapter 20.25.070 BMC. Additional design standards specific to the Samish Way Urban Village apply per BMC 20.37.130(C).
8. Applicable standard development regulations are found in BMC 20.37.100-.170. There is no specified minimum lot size, density limit, lot coverage or open space requirements for the subject site.
9. Street front commercial uses are required along the N. Samish Way frontage per BMC 20.37.130(C)(1). Office uses are proposed to front N. Samish Way in Phase 1 as permitted in BMC Table 20.37.120.

Pre-Application

10. On December 13, 2016, the applicant completed a pre-application conference with City staff (PRE2016-0085). Per BMC 21.10.170(E), if a complete application is not submitted within one year of the preapplication conference, a new preapplication conference or written waiver is required.
11. On February 1, 2019, the application for Multimodal Transportation Concurrency was issued by the city (CON2019-0003) per Chapter 13.70 BMC.
12. On February 6, 2019, a subsequent preapplication conference (PRE2019-0013) was waived on the basis that the proposed project remained substantially the same as that reviewed in 2016 under PRE2016-0085.
13. On February 6, 2019, the applicant applied for a Pre-Application Design Guidance Public Meeting (PRE2019-0019) with the Design Review Board (DRB).
14. On February 28, 2019, notice for the Pre-Application Design Guidance Public Meeting (PRE2019-0019) was mailed pursuant to BMC 21.10.200 to all property owners within 500 feet of the subject site and the Sehome and York Neighborhood representatives. Notice of the public meeting was also published on the City's website and in the Bellingham Herald.
15. On March 19, 2019, the DRB reviewed the proposal at the Pre-Application Design Guidance Meeting. The DRB provided guidance on which UVDS were applicable, how the design standards, guidelines, and regulations apply to the proposed project, and information that should be provided with the design review application.

a. Specific guidance from the DRB was as follows:

- 1) Use high quality material that would achieve depth and texture.
- 2) Stair towers:
 - i. Treatment of blank walls generally not as much an issue since so much else going on with the building.
 - ii. The panel siding seams should provide a measure of depth and texture.
 - iii. Consider adding a ribbon of glass visible from Samish Way on the smaller tower similar to the ribbon on the larger tower.
- 3) Add kitchen windows to the elevation in Figure 1.4 in the DRB staff report.
- 4) Add bathroom windows to the elevation in Figure 1.1 of the DRB staff report (misabeled as kitchen windows in Figure 1.1)

- 5) Take a look at the window and floor plan options for the corner units on Figure 1.1 with intent to provide more visual emphasis to the building module corner and increase livability of the corner units.
- b. **Departures from design standards.** Staff identified that design departures would be required from the following:
 - 1) BMC 20.25.070(B)(1)(b)(ii) for interim use of existing open parking adjacent to Laurel Street and N. Samish Way.
 - 2) BMC 20.25.070(B)(1)(f)(ii) to not provide a pedestrian connection through the site given the grade change, intent to develop a raised central courtyard atop the structured parking lid with private usable space, and to provide access control and security. The provision is generally not applicable to the site given its triangular shape, and it does not consist of an oversized “super block” such as that found along Samish Way south of Abbott Street.
 - 3) BMC 20.25.070(B)(2)(f)(iii) to reduce the width and increase the height of the awnings associated with the commercial portion of the building from what is otherwise required. The awnings as proposed are intended as an integral architectural element of the building highlighting the corner office module.

Staff informed but did not seek the Board’s guidance for the departures above because staff determined the interim and alternative designs would “provide an equal or better solution” to the applicable standards and fulfill the criteria in BMC 20.25.070(A)(2) for obtaining a design departure. The Board deferred to staff’s recommendation and elected not to discuss these additional departures.

- 4) The applicant requested a design departure from (BMC 20.25.070(B)(2)(g)(ii) to allow a temporary blank wall on the 1st and 2nd floors of the south elevation perpendicular to N. Samish Way (Figure 1.2 in the DRB staff report) given the wall is temporary and will be covered when Phase 2 is constructed. The Board recommended approval of the departure since it will be temporary, and finding that the proposed interim stairs will provide sufficient visual interest to “provide an equal or better solution” to the applicable standards and fulfill the criteria in BMC 20.25.070(A)(2) for obtaining a design departure.
16. **Staff included the following design guidance following the DRB meeting:** Strengthen the tops of the building’s book-end masonry modules (one or both) on Otis Street consistent with BMC 20.25.070(B)(2)(a)(ii). This could potentially be accomplished with a change in brick color, reveal, and/or pattern, adding a cornice, etc.
17. On March 27, 2019, the applicant held a pre-application neighborhood meeting consistent with BMC 21.10.180. Approximately 13 members of the public attended and provided multiple comments and inquiries about the design.

Applications and Process

18. On May 28, 2019, the applicant submitted applications for Design Review (DR2019-0003) (**Exhibit A**), Parking Reduction (ADM2019-0017) (**Exhibit B**), and a SEPA Checklist (SEP2019-0020) with supporting reports.
19. The parking reduction application is a request to reduce the number of required parking spaces for the tenants and the commercial uses in **Phase 1** from 95 to 72 spaces, a reduction of 23 parking stalls (24.2 percent). Pursuant to BMC 20.27.150(E), the PCDD Director may administratively reduce parking up to an additional 30 percent for uses that

are inherently less auto dependent, and for mitigation provided in lieu of the parking reduction. The proposed mitigation must be demonstrated to the satisfaction of the Director, to be adequate and proportionate to the requested parking reduction. Included in the application were supporting information titled Reduction Analysis (**Exhibit B**).

20. A parking reduction is a Type II decision in BMC 21.10.040 and is referred to therein as a parking waiver. It is a Type II decision because a SEPA threshold decision is required for the project. Throughout this permit the parking waiver is referred to as a parking reduction, as stated and authorized in BMC 20.37.150(E).
21. On May 22, 2019, the applicant submitted a letter requesting three (3) minor modifications relating to adding street parking and enhanced landscaping along Otis Street pursuant to BMC 20.37.140(C). BMC 20.37.140(C) states:

Street standards shall be consistent with the Samish Way subarea plan streetscape designs. Minor modifications may be approved by the planning and public works directors. Such modifications may be granted when practical difficulties arise in the design and construction of streets due to topographic geological limitations or other problems inherent or peculiar to the area, or where the directors find that imposition of the required street design would be detrimental to the interest of the neighborhood.

The minor modifications consisted of the following, including staff analysis and recommendations:

- a. Exception #1: Along the stretch of Otis Street where parallel parking is proposed, lengthening the tree wells from the 5' x 5' standard to 5' x 18' as proposed within the 12' wide sidewalk cross section is acceptable to provide additional curbside landscaping and pedestrian separation from the parking. Sidewalk extensions would be included between the parallel parking and the sidewalk to facilitate the transitions from parked cars to the sidewalk. This would leave a 7' wide unobstructed sidewalk for pedestrian circulation and allow larger planting beds for enhanced tree health and pedestrian comfort.
 - b. Exception #2: A 9' wide sidewalk with no curbside street trees is proposed generally along the stretch of Otis Street where perpendicular parking is proposed to serve the BHA administrative offices. The proposal is viewed as acceptable given there is no building entry on this section of sidewalk other than the garage entry at the west end, landscaping will be on the opposite side of the sidewalk from the perpendicular parking and at either end of the parking, and even with vehicular overhang this sidewalk section will transition to the 7' sidewalk per Exception #1 above.
 - c. Exception #3: Perpendicular parking is proposed to serve the BHA administrative offices as detailed in Exception #2 above. This request for 90-degree head-in parking near the intersection with N. Samish Way is not acceptable due to its proximity to the principal arterial and potential conflicts with vehicle queuing and turning movements. This parking should be reconfigured as angled parking.
22. The requested street design modifications as conditioned above should be approved as they are generally consistent with the applicable criteria in that the surrounding area suffers from an inherent lack of sidewalks and public street parking. The modifications are consistent with adopted policies in the SWUV to provide ample on-street parking, sidewalks that are safe and attractive, and increased landscaping where opportunities exist. The

modifications should be submitted and reviewed for final determination with a Public Facilities Construction Agreement.

23. On June 21, 2019, the applicant submitted an updated SEPA Checklist.
24. On June 24, 2019, a Notice of Incomplete Application (NIA) was sent to the applicant for the parking reduction, design review requirements, and general comments on the proposed buildings' compliance with building, fire and public works code requirements (**Exhibit E**).
25. On July 22, 2019, the applicant submitted a response to the NIA (**Exhibit E**).
26. On August 7, 2019, a Notice of Complete Application, Application, and Optional DNS Process was mailed to all property owners within 500 feet of the subject property, Sehome and York Neighborhood representatives, and applicable review bodies on the City's SEPA mailing list. Notice and posting complied with Bellingham Municipal Code (BMC) 21.10. The last date of comment was August 20, 2019.
27. In response to the notice, the city received 5 comments (**Exhibit F**) expressing concern about the project relating to it being too large (mass and density), lack of available street parking in the area, and the project's size exacerbating existing traffic congestion on Samish Way. The project height, mass and density comply with adopted development standards. The project will be adding 14 new improved public curbside street parking stalls and new sidewalk along the project's Otis Street frontage where none currently exists which will add to on-street public parking capacity in the area. The Public Works Department evaluated the proposal and determined that a traffic impact analysis is not required.

Staff evaluated the public comment and determined that the impacts resulting from the project's size/scale, infrastructure construction, and traffic generation are consistent with adopted development standards and city policies. The impacts were determined to be non-significant and do not warrant mitigating measures.

28. On October 22, 2019, a Determination of Non-Significance (**SEP2019-0020**) was issued by the SEPA Official.

Design Review and Development Regulation Analysis

29. Staff believes that the site and elevation plan and renderings adequately comply with the standard development regulations in BMC 20.37.100-170 as follows:
 - a. This site is located in the SWUV's Commercial Transition 2 (CT2) area (BMC Figure 20.37.110). The proposed uses, residential and office, are allowed in this area. There is no specified minimum lot size, density limit, lot coverage or open space requirements for the subject site.
 - b. This proposal meets the SWUV's height and floor area ratio (FAR) requirements: It is approximately 55 feet high from average grade plane (maximum height is 75 feet) and it has a FAR of approximately 2.0 (maximum FAR is 3.5) on the Phase 1 site.
 - c. The proposal meets the SWUV's setback requirements applicable to commercial zoned land when across the street (Otis Street) from residentially zoned land.
 - d. Development having a FAR of 2.0 or greater is requires a minimum Green Area Factor landscaping score of 0.20. The preliminary landscape plan (**Exhibit C, Sheet 106**) shows that the required landscaping can be provided on this site. A final landscape plan demonstrating compliance with this requirement should be submitted for review and approval with the building permit.

- e. A total of 28 bike parking stalls are being provided - 13 bike parking stalls more than the 15 required by BMC 20.37.150(F).
 - f. Auto parking is addressed separately in findings of fact #s **33-38** below.
 - g. Street Improvements. Pursuant to BMC 20.37.140(C), street improvements are required for all abutting street frontages of the project site, and they are to be consistent with the streetscape designs in the Samish Way Urban Village Subarea Plan. The City commissioned a comprehensive corridor study to examine alternatives for the Samish Way streetscape which was completed in 2016. The study recommended that Samish Way be reduced to one vehicle lane in each direction, add buffered bicycle lanes in each direction, and add a landscape median on Maple Street between Otis and Edwards Streets. This work to modify the travel lanes between the existing street curbs will be undertaken by the City as funding becomes available. Accordingly, street improvements required for the project should be consistent with the Samish Way Subarea Plan streetscape designs except as follows:
 - 1) Consistent with the above, improvements to N. Samish Way should be limited to replacing the sidewalk along the Phase 1 and 2 street frontage between the existing street curb and the property line with a 12' wide sidewalk and minimum 5' x 5' curbside street tree wells, and additional work as necessary for installation of utilities. Street tree wells may be lengthened to 13' as proposed on **Exhibit C, Sheet L106** for added landscaping and improved street tree health.
 - 2) Improvements to Otis Street should be consistent with the Type II non-arterial street cross section with minor modifications as recommended in findings of fact **#21** and **22** above, including a bulbed-out intersection at Laurel Street as shown on **Exhibit C, Sheet C103**.
 - 3) The designs for street improvements should be submitted and reviewed for final determination with a Public Facilities Construction Agreement prior to or concurrent with building permit application.
 - 4) The design for interim vehicular access to the site from Laurel Street should be submitted and reviewed for final determination with a Public Facilities Construction Agreement prior to or concurrent with building permit application.Prior to or concurrent with building permit application, the developer may submit a request for street modifications per BMC 20.37.140(C) along with a written phasing plan for the following:
 - 5) Defer design and improvements to the Phase 1 frontage along Laurel Street to Phase 2 provided the work is bonded for in the interim.
 - 6) Defer design and improvements to the Phase 2 frontages along the remainder of Laurel Street, Abbot Street, and N. Samish Way to be concurrent with buildout of Phase 2.
 - 7) Bonding for deferred street improvements may be required.
30. Staff reviewed the Design Review application and the applicant's response to the July 22, 2019 Notice of Incomplete Application and found that the project's design largely incorporated the Design Review Board's guidance from the March 19, 2019 Design Guidance Meeting outlined in **#15** above and the UVDS. Staff supports the site plan and building design, and recommends approval of the design submitted, provided certain

specific design elements are addressed. Specific observations and recommendations include:

- a. Design Departures in finding of fact **#15(c)(1-3)** above should be approved consistent with the justification therein. Concerning the interim use of existing open parking adjacent to Laurel Street and N. Samish Way, landscape screening should be applied consistent with BMC 20.25.070(B)(1)(b)(iii). A final landscape plan demonstrating compliance with this requirement should be submitted for review and approval with the building permit.
 - b. The requested design departure in **#15(c)(4)** above (from BMC 20.25.070(B)(2)(g)(ii)) for a temporary blank wall on the 1st and 2nd floors of the south elevation perpendicular to N. Samish Way is no longer necessary. The subject elevation was modified to include openings to accommodate internal access with Phase 2 and will be filled with storefront glazing in the interim.
 - c. A larger building setback is proposed at the intersection of N. Samish Way and Otis Street to provide a wider sidewalk and public space consistent with BMC 20.25.070(B)(1)(a)(ii).
 - d. Rooftop Screening, Site Lighting, and Signs as regulated by BMC 20.25.070(B)(1)(e) and (h) and 20.37.170, are not addressed by this application and should be conditioned with this permit to be evaluated at time of building permit.
31. There are no known critical areas on or adjacent to the site.
32. The application materials included preliminary engineered drawings. Those drawings were used as a reference documents for review of the land use applications but are not approved as a part of this permit and have not been reviewed for compliance with the Bellingham Municipal Code. The engineered drawings included a preliminary Storm Report and Civil Plans were provided with the application materials. Stormwater generated from the site will be treated and detained on site and directed to a new stormwater main proposed with this project in Otis Street and also to an existing storm water main within the N. Samish Way right-of-way as demonstrated on sheets C103 and C105 of PFC2019-0026.

Parking Reduction Analysis

33. Pursuant to BMC 20.27.150(E), the PCDD Director may administratively reduce parking up to an additional 30 percent for uses that are inherently less auto dependent, and for mitigation provided in lieu of the parking reduction. The proposed mitigation must be demonstrated to the satisfaction of the Director, to be adequate and proportionate to the requested parking reduction.
34. The proposed residential use requires 72 parking stalls and the proposed office use requires 23 parking stalls for a total of 95 required parking stalls. 72 parking stalls are proposed to be developed on site for the project in two levels of structured parking which is would be an overall 24.2% reduction for the mixed office and residential uses.
35. The applicant is requesting a full 30% reduction from the "residential" parking requirement for providing all affordable housing as authorized in BMC 20.37.150(E) which would allow a 21-stall reduction (rounded down from 21.6). Two (2) additional parking stalls must be accounted for the overall 24.2% reduction. The applicant is proposing to provide additional mitigation as follows:
- a. 14 new improved public curbside street parking stalls and new sidewalk along the project's Otis Street frontage where none currently exists.

- b. 4 motorcycle parking stalls within the parking garage.
 - c. 13 bike parking stalls more than the 15 required by BMC 20.37.150(F) – (28 total provided).
 - d. 13 temporary parking stalls on the existing adjacent parking lot until construction begins on Phase 2.
36. The improvement of new “public” curbside street parking on Otis Street where none currently exists is especially beneficial to the project site and the surrounding neighborhood as it adds to the inventory of public street parking available to all users. Unlike reserved on-site parking, curbside street parking is highly efficient as when stalls are vacated, they become immediately available for use by tenants of the site and adjacent users. Increased accommodations for motorcycles and bikes increase options for building tenants who choose or rely on a car-free lifestyle. Also notable, the required on-site office parking may benefit from utilization at a different time than the required residential parking.
 37. Whatcom Transit directly serves the site on N. Samish Way with Route 107 and operates a high frequency GoLine approximately ¼ mile south on Bill McDonald Parkway. Both provide direct access to WWU and the Downtown. Sehome Village Shopping Center is just over ¼ mile south and provides multiple services, retail and groceries. Together, the proposed mitigation and surrounding amenities and opportunities support housing and lifestyles that are less dependent on the auto or car free.
 38. **Parking Reduction Recommendation:** Based on the proposed affordable housing and mitigation, and factors surrounding the site, the parking reduction for **Phase I** should be granted. The reduction should be conditioned to be coordinated with Phase II such that any subsequent parking reduction for Phase II would mitigate and manage parking on a project wide basis.
 39. Any Finding of Fact that should be denominated a Conclusion of Law shall be deemed a Conclusion of Law. Any Conclusion of Law that should be denominated a Finding of Fact shall be deemed to be a Finding of Fact.
 40. Staff recommends, and the PCDD Director finds, that **Phase 1** of the proposed development, as conditioned in **Section IV** herein, conforms to the requirements of Chapters 20.25 and 20.37 BMC, and this combined Design Review and Parking Reduction Permit should be approved.
 41. The PCDD Director is authorized to grant urban village design review approval under BMC 20.25.020(D), and a parking waiver (reduction) under BMC 21.10.040(B)(12).

IV. DECISION AND CONDITIONS

Based upon the Findings of Fact and Conclusions, the PCDD Director, or designee, approves this combined Design Review and Parking Reduction Permit, for the plans in **Exhibit C**, and the information in **Exhibits A** and **B**, subject to the following conditions:

A. General

1. All development and use of the property legally described in **Section I** of this permit shall be generally consistent and to the level of detail shown with the permit plans and other exhibits listed in **Section II**, except as necessary to comply with the requirements in the Bellingham Municipal Code and the conditions of this permit.

2. Development of the property shall be consistent with all applicable provisions of the BMC and does not excuse the applicant from compliance with any other federal, state or local statutes, ordinances or regulations that may be applicable to this project. All building and construction permits shall be consistent with the provisions of this permit. All work shall be completed according to this permit.
3. Concurrent with a Building Permit Application, site and building plans in compliance with this permit shall be submitted for review and approval.
4. Site disturbance, including but not limited to clearing and grading, is not permitted without an issued building permit, except as authorized for herein.
5. A Public Facility Construction Agreement shall be obtained from the City for installation of any necessary public infrastructure prior to or concurrently with a building permit application.
6. The City may impose additional conditions if found that sufficient information was not present with the applications to comply with the BMC.
7. Prior to approval of any building or construction permits, the City shall determine compliance with the terms and conditions of this permit.
8. In the event the owner/applicant/assigns fails to comply with the terms of the conditions herein, the permit may be rescinded.

B. Design Review and Land Use

1. Departures from the following design standards are hereby granted, consistent with the findings and recommendations in subsection **III(15)(b)** above:
 - a. BMC 20.25.070(B)(1)(b)(ii) for interim use of existing open parking adjacent to Laurel Street and N. Samish Way.
 - b. BMC 20.25.070(B)(1)(f)(ii) to not provide a pedestrian connection through the site
 - c. BMC 20.25.070(B)(2)(f)(iii) to reduce the width and increase the height of the awnings associated with the commercial portion of the building from what is otherwise required.
2. Panel siding seams should provide a measure of depth and texture on the stair tower modules consistent with BMC 20.25.070(B)(2)(g) to minimize the blank wall effect.
3. A Master Lighting Plan (MLP) shall be submitted with the building permit application for review and approval by the City. All outdoor lighting shall be sized and directed to avoid adverse impacts and spillover onto adjacent properties. Any up lighting shall be directed at the building and minimize spillover that would contribute to night sky light pollution. The MLP shall include the following information.
 - a. A photometric site plan, drawn to scale, showing all buildings and parking areas, fixture and pole height, and include all proposed exterior lighting fixtures and foot-candle spread.
 - b. Design specifications for all proposed exterior lighting fixtures shall include photometric data, cutoff devices, bulb wattage/type, and other descriptive information.
 - c. The lighting must also be, as much as physically possible, contained to the site area except where lighting is intended for public sidewalks.
4. All exterior mounted mechanical equipment (including venting, gas meters and rooftop) shall be identified on the planset submitted for building permit application. If such equipment is used, mitigate impacts to adjacent public sidewalks through screening, architectural manipulation, or similar strategies. Mitigating measures are subject to approval by the Director.
5. Proposed signage regulated by BMC 20.37.170 shall require a separate sign permit.

6. A Landscape Plan showing compliance with a Green Factor Landscape score of 0.20 shall be submitted with the building permit application for review and approval by the City. The Landscape Plan shall be generally consistent with the following:
 - a. Landscape beds shall include trees and a mix of small, medium and large plantings for a “layered” appearance, except where landscaping standards require a hedge, or an alternative design is approved by the Planning Director. Select species that are appropriate for the site and site uses, and low maintenance. Drought tolerant species shall be used for small ornamental trees, shrubs and ground covers unless an irrigation system is provided.
 - b. Proposed street trees located in public rights-of-way will require a Street Tree Permit. Application must be made prior to issuance of building permit and include a utility conflict plan to ensure street trees will not impact utilities in the right-of-way.
 - c. Street trees shall be sized and located such that they minimize interference with building entries, vision clearance for the driveway and required fire truck staging areas.
 - d. Concerning the interim use of existing open parking adjacent to Laurel Street and N. Samish Way, landscape screening shall be applied consistent with BMC 20.25.070(B)(1)(b)(iii).
 - e. Landscaping materials, species and design, including lighting, should be consistent with CPTED principals to define space, deter cut through and trespass, provide visibility with the 3' - 7' rule, and provide for ongoing maintenance.
 - f. If landscaping is not installed at time of Certificate of Occupancy, a 2-year maintenance surety acceptable to the City shall be provided at 50% of the value of the landscaping materials and installation, based on a bona fide independent bid.
7. A Stormwater Site Plan (SSP) shall be required in conformance with BMC 15.42 and the most recent version of the Department of Ecology (DOE) stormwater management manual.
8. Except as provided in #9 below, street and sidewalk improvements shall be consistent with the findings and recommendations in subsection **III(29)(g)** above, including repair and replacement. These improvements will require a Public Facilities Construction Agreement that must be issued prior to or concurrently with a building permit application.
9. Requested modifications to street standards are conditionally approved consistent with the findings and recommendations in subsection **III(21-22)** above. The modifications shall be submitted and reviewed for final determination with a Public Facilities Construction Agreement.
- C. Parking Reduction.** The parking reduction for Phase I is hereby approved and shall be coordinated with Phase 2 such that any subsequent parking reduction granted for Phase II will mitigate and manage parking on a project wide basis.
- D. Archaeological Resources:** In the event that archaeological resources are discovered during any ground disturbing activity at the site, work should be halted immediately, and notification made to affected/concerned tribes and Washington State Department of Archaeology and Historic Preservation (DAHP).
- E. Occupancy:** Prior to issuance of a Certificate of Occupancy for any building, all terms and conditions of this permit shall be met. The applicable department may require bonding if the Developer, prior to completion of the terms of this permit, requests Temporary Occupancy Permits. Certificate of Occupancy permits shall be processed in accordance with BMC 17.10.020.

- F. This application is subject to the applicable requirements contained in the Bellingham Municipal Code, and Fire and Building Codes. It is the responsibility of the applicant to ensure compliance with the various provisions contained in these codes. Any element(s) of the project design that is conditioned in the design review approval, but not in compliance with other applicable codes, may be subject to design review in the event correcting such element(s) requires redesign.

V. AMENDMENTS

Amendments to this combined Design Review and Parking Reduction Permit may be requested by the applicant or owner and approved by the PCDD in writing, provided such amendments to not substantially change or alter major elements of the project.

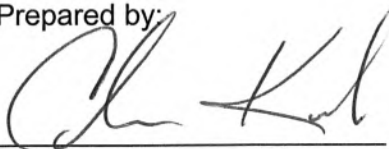
VI. EXPIRATION

In accordance with BMC 21.10.260, this combined Design Review and Parking Reduction Permit is valid for two years from the date of issuance unless a completed building permit application, with the other required concurrent submittals, is submitted and determined to be complete prior to such date.

VII. APPEAL


Pursuant to BMC 21.10.110(K), this combined permit may be appealed within 14 days from the date of the Notice of Decision to the City's Hearing Examiner. Procedures for appeal to the Hearing Examiner are contained within BMC 21.10.250. Any appeal must be filed with PCDD on the appropriate forms and be accompanied by a filing fee as established by the City Council prior this established appeal period.

Prepared by:



Chris Koch, Planner II

DEPARTMENTAL APPROVAL:



Kurt Nabbefeld,
Development Services Manager
Planning and Community Development Department

10-22-2019

Date of Approval



EXHIBIT A

Permit Center

210 Lottie Street, Bellingham, WA 98225

Phone: (360) 778-8300 Fax: (360) 778-8301 TTY: (360) 778-8382

Email: permits@cob.org Web: www.cob.org/permits

Land Use Application

Check all permits you are applying for in the boxes provided. Submit this application form, the applicable materials listed in the corresponding permit application packet(s) and application fee payment.

<input type="checkbox"/> Accessory Dwelling Unit <input type="checkbox"/> Binding Site Plan <input type="checkbox"/> Clearing Permit <input type="checkbox"/> Conditional Use Permit <input type="checkbox"/> Critical Area Permit <input type="checkbox"/> Minor Critical Area Permit <input checked="" type="checkbox"/> Design Review <input type="checkbox"/> Grading Permit <input type="checkbox"/> Home Occupation <input type="checkbox"/> Institutional <input type="checkbox"/> Interpretation <input type="checkbox"/> Landmark – Historic Certificate of Alteration <input type="checkbox"/> Legal Lot Determination <input type="checkbox"/> Nonconforming Use Certificate	<input type="checkbox"/> Parking Adjustment Application <input type="checkbox"/> Planned Development <input type="checkbox"/> Rezone <input type="checkbox"/> SEPA <input type="checkbox"/> Shoreline Permit <input type="checkbox"/> Shoreline Exemption <input type="checkbox"/> Subdivision-Short Plat/Lot Line Adjustment <input type="checkbox"/> Subdivision-Preliminary Plat <input type="checkbox"/> Subdivision-Final Plat <input type="checkbox"/> Variance <input type="checkbox"/> Wireless Communication <input type="checkbox"/> Zoning Compliance Letter <input type="checkbox"/> Other: 19,049 SF	Office Use Only Date Rcvd: _____ Case #: _____ Process Type: _____ Neighborhood: _____ Area Number: _____ Zone: _____ Pre-App. Meeting: _____ Concurrency: _____
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Project Information

Project Address 301 & 315 N. Samish Way, Bellingham, WA Zip Code 98225

Tax Assessor Parcel Number (s) 380331425243 & 380331421560000

Project Description Phased development of the former Aloha! Motel site into a mixed use multi family project consistent with the City's vision for the Samish way urban village subarea plan.

Applicant / Agent ☒ Primary Contact for Applicant

Name RMC Architects, Robert Wright

Mailing Address 1223 Railroad Ave.

City Bellingham State WA Zip Code 98225

Phone 360.676.7733 Email robert.w@rmcarchitects.com

Owner (s) ☐ Applicant ☐ Primary Contact for Applicant

Name Bellingham Housing Authority, Ken Van Winkle

Mailing Address PO Box 9701

City Bellingham State WA Zip Code 98227-9701

Phone 360.676.6887 Email kvanwinkle@bwcha.org

Property Owner(s)

I am the owner of the property described above or am authorized by the owner to sign and submit this application. I grant permission for the City staff and agents to enter onto the subject property at any reasonable time to consider the merits of the application and post public notice. I certify under penalty of perjury of the laws of the State of Washington that the information on this application and all information submitted herewith is true, complete and correct.

I also acknowledge that by signing this application I am the responsible party to receive all correspondence from the City regarding this project including, but not limited to, expiration notifications. If I, at any point during the review or inspection process, am no longer the Applicant for this project, it is my responsibility to update this information with the City in writing in a timely manner.

Signature by Owner/Applicant/Agent  Date 5/13/2019

City and State where this application is signed: Bellingham WA
City State



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DESIGN REVIEW CHECKLIST

(PLEASE PRINT CLEARLY OR TYPE IN BLUE OR BLACK INK)

To assist staff in review of a design review application, the following items must be complete.

Planning staff will help you determine which of these requirements are applicable. See instructions in this packet.

COMPLETE THIS CHECKLIST:

- ☐ Pre-application conference or waiver (if required)
- ☐ Pre-application Design Guidance Meeting by the Design Review Board
- ☐ Pre-application neighborhood meeting or waiver (if required)
- ☐ Transportation certificate of concurrency (if required)
- ☐ Land Use Application (attached)
- ☐ Environmental checklist (SEPA) (if required)
- ☐ Statement describing the development objectives.
- ☐ A project narrative that includes:
 - ☐ A detailed description of how the project meets each applicable design standard and guideline for the design area (Urban Village, etc.). For example, the narrative for a new building downtown would include a list of each applicable standard and guideline and how the project addresses each item. Print N/A for those standards/guidelines that don't apply.
 - ☐ A detailed description of how the project addresses any design guidance provided by the DRB or HPC.
 - ☐ Any requested departures from the design standards, along with an explanation of how the departure criteria is met.
- ☐ Residential Multi Data sheet (attached)
- ☐ Photos of the site context showing the subject site and adjacent buildings, including the block face and the block face across the street
- ☐ Zoning compliance data (calculate required parking, lot coverage, open space, etc.)
- ☐ List new gross floor area: Ground floor: 19,049 SF Upper floors total 68,271 SF
- ☐ Value of new construction and exterior alterations: ☐ \$50,000 or less ☒ Over \$50,000
- ☐ Legal description of the property
- ☐ Other land use permit applications required for the project
- ☐ One (1) full-size (24" x 36") scaled, dimensioned plan set (more may be requested by staff)
- ☐ Eight (8) sets of 11" x 17" reductions of all plans, including colorized sheets
- ☐ Provide copies of the plans in an electronic format.

- ☒ Application fee (DRB pre-application review fee credited if application is filed within 6 months of the DRB meeting)
 - ☒ Mailing list and labels (if Type II or requires Board or Commission review)
 - ☐ Submit information if the proposal is utilizing Transfer of Development Rights (TDR) / Purchase of Development Rights (PDR). Indicate the number to be transferred or purchased. Note that TDR/PDR may only be used within a City TDR/PDR receiving area.
 - ☒ Other required information: Master Plan Documentation
-
-

SITE PLAN REQUIREMENTS CHECKLIST:

Provide a site plan containing the following information. This checklist is divided by type of development. Provide the information under each heading that applies to your project.

The Planning & Community Development Department may require additional information to evaluate the proposal.

Scale shall be standard architectural or engineering. The scale must allow clear depiction of all required information, typically between $\frac{1}{4}" = 1'$ and $\frac{1}{8}" = 1'$; and $1" = 10'$ and $1" = 40'$;

Existing Site Conditions

- ☒ Vicinity map
- ☒ All lot lines and site boundary dimensions
- ☒ Topography
- ☐ Critical Areas
- ☒ Streets, alleys, etc.
- ☒ Utilities
- ☐ Location, dimensions, gross floor area and use of existing structures. Indicate all structures to be demolished
- ☒ Distances between structures and property lines
- ☒ Location and design of all paving
- ☒ Walkways and bicycle paths

New Development

- ☐ Proposed lot lines
- ☐ Existing and proposed easements
- ☐ Proposed grading/final contours
- ☐ Location, dimensions, and use of proposed buildings and structures. Include decks, patios, fences and signs
- ☐ Distances between proposed buildings, parking areas and property lines
- ☐ Existing and proposed elevation contours at intervals of not greater than 5 feet. Provide 2-foot contours when requested by the Planning & Community Development Department
- ☐ Location, height, top elevation and width and design of existing and proposed retaining walls and rockeries. Cross sections may be required

- ☐ Location, dimension and number of parking spaces (including accessible spaces), bicycle parking, drop-off areas and driveway access
- ☐ Proposed and existing pedestrian walkways and surfacing
- ☐ All significant man-made or natural features (ponds, streams, etc.)
- ☐ Proposed storm water detention and treatment features (swales, ponds, vaults, etc.) Size, location and type of facilities on this preliminary plan shall be consistent with City, State and Federal requirements for storm water management.

(Note: The preliminary plan shall be designed or reviewed by a Civil Engineer registered with the State of Washington for conformance with regulations. The City may ask for information to corroborate the preliminary design. Any regulatory deficiencies in the proposed storm water plan are wholly the responsibility of the proponent and/or the engineer. You may be required to apply for amendments to land use and design permits if there are changes in the final storm water facilities that affect the site design.)

- ☐ All existing street and alley rights of way abutting the site. Include street name, width of right of way and location of existing improvements such as sidewalk, curb, medians, bus stops, overhead utility lines, street trees, street lights and street improvement width. Note any proposed changes to the street design
- ☐ Location of existing and proposed utilities within the site such as sewer, water, gas and electricity
- ☐ Location of existing fire hydrants
- ☐ Proposed garbage/recycling collection areas and dimensions
- ☐ Location and type of proposed exterior lighting, including parking lot and building
- ☐ Location of proposed signs
- ☐ Land uses, lot lines and approximate location of structures and pavement on abutting properties

Conceptual Landscaping Plan

- ☒ Scale
- ☒ Abutting street right of way to the curb
- ☒ Location, height, and designs of rockeries, retaining walls and fences
- ☒ Surface and subsurface stormwater facilities
- ☒ Buildings and paving
- ☒ Topographic contours - pre and post construction
- ☐ Wetlands, ponds, streams and proposed buffers
- ☐ Existing vegetation to be retained
- ☒ General location and type of proposed trees, shrubs and ground cover

Commercial or Industrial Development

- ☐ Identification of service truck access routes
- ☐ Loading areas
- ☐ Outdoor storage or display areas
- ☐ Location and type of screening and buffering proposed

Critical Areas on or Adjacent to the Site

- ☐ Areas of flood hazard (FEMA floodplain or floodway)
- ☐ Wetlands on site and within 100 feet of the project boundary
- ☐ Streams, ponds or lakes on site and within 200 feet of the project boundary



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MULTIFAMILY RESIDENTIAL PLAN DATA SHEET

Provide the following information for the proposed development. Use definitions and calculation methods in BMC Title 20 Land Use Development.

If the project is located in a Planned land use district (Use Qualifier) or has applied for a City of Bellingham multifamily tax exemption, enter the development contract or application number:

Not applicable

Contract/application number: _____

Total number of units: 69

	Studio	1-bedroom	2-bedroom	3-bedroom	4-bedroom
Existing Units	0	0	0	0	0
Proposed Units	26	30	8	5	0
Demolished Units	- 0	- 0	- 0	- 0	- 0
Total Net Units	26	30	8	5	0

Total site area: 93,755 sq. ft. Proposed Density: 639 sq. ft. of land area per dwelling unit

Lot Coverage: 25,044 sq. ft. 57.2 % of site

Open Space: 18,711 sq. ft. 42.8 % of site

Total useable space: 26,925 sq. ft.

Private: 1,867 sq. ft. Common: 25,058 sq. ft.

Describe type of useable space provided (decks, yards, recreation buildings, type of recreational facilities and equipment): Podium deck, private yards, landscaped yard

Total number of parking spaces proposed: 83

Garage parking spaces proposed: 70

Carport parking spaces proposed: 0

Check Yard Setback Options Taken:

☒ None

☐ Side yard window wall

☐ Rear yard window wall

☐ Side yard wall without window

☐ Rear yard wall without window

Height of tallest building: 62' feet under definition #1

Height of tallest building: 43.4' feet under definition #2

57.67' above avg. grade to top of parapet

SAMISH WAY REDEVELOPMENT (ALOHA!)

DESIGN REVIEW

DEVELOPMENT OBJECTIVES

The proposed Samish Way Redevelopment project is located on a site formerly occupied by the Aloha Motel in the Samish Way Urban Village. The City acquired the property through condemnation action, and offered it to potential developers in an RFP process. The Bellingham Whatcom County Housing Authority was selected as the developer. Their development strategy includes a mixed-use building with commercial uses facing Samish Way, townhouses facing the residential neighborhood along Otis Street, and three multi-family buildings grouped around a central courtyard on top of two stories of structured parking. A total of 160 residential units is proposed, including senior housing, work force housing, and other affordable units serving residents with incomes between 30% to 80% AMI.

The development is intended to be in full compliance with the standards set forth in the Samish Way Urban Village Subarea Plan and be built in two phases. Phase I includes Building A located on the north side of the property bordering Otis Street. Phase II includes Building B and C on the southern part of the property. For this review, we would appreciate high level and cursory comments on the Master plan, with a more detailed review of our phase 1 submission.

	Phase I	Phase II
Residential Units	49,702 SF	78,500 SF
Commercial Space	11,000 SF	4,700 SF
Parking	26,618 SF	36,400 SF
SUBTOTAL	87,320 SF	119,600 SF

Samish Way Redevelopment

315 North Samish Way

7.16.2019

Project Narrative:

This document discusses how phase 1 of the project relates to the Urban Village Design Guidelines (BMC 20.25.070 & BMC 20.37.130[c][1-3]) and identifies any potential need for departures. We have only analyzed criteria that pertains to our project type.

20.25.070 Urban Village

B. Specific Standards

1. Site Design

a. Orientation to Street

- ii. *Guideline.* Locate the building at sidewalk edge. A larger setback may be considered in order to accommodate a wider sidewalk, public space, or outdoor cafe.

Response: The proposed development employs a zero lot line along Samish Way, where the commercial portion of the development interfaces with the street. The Otis Street and Laurel Street facades are set back from the property line and sidewalk edge because there are residential units near grade level.

- ii. *Guideline.* Locate the primary entrance at street level facing a public street. The primary entrance should be more articulated and highlighted (size, material, recessed, lighting) than secondary entrances.

Response: The primary entrance for the commercial space (BHA Offices) occurs on Samish Way at street level facing the street & landscaped lot corner. This entrance is articulated with large glazed storefront panels and a roof form that is higher than the remainder of the building. Appropriate lighting will be implemented to highlight this hierarchy. The primary entrance for the residential portion of the building occurs along Otis street and is highlighted by the interruption of the building plane with storefront glazing extending up 4 levels and an entry canopy.

b. Surface Parking

- ii. *Standard.* Parking shall be located to the rear or side of a building and shall not be located at intersections.

Response: All of the parking for phase 1 is in the structured parking which is buried behind residential and commercial uses and not visible from the street. However, an existing temporary surface parking is proposed to be utilized on the adjoining lot as an interim measure between the completion of phase 1 and the beginning of phase 2. A parking agreement will be in place for use of the existing parking. Due to the triangular shape of the lot and flanking streets on all sides, the convention of rear and side locations become unclear. However, the existing parking is near an intersection, and for this reason we are seeking a design departure from this standard with the understanding that this parking is temporary.

iii. Standards.

- (A) A wall or evergreen hedge designed to be maintained at a height of at least two and one-half feet and not more than three and one-half feet is required along the street frontage of any street level open parking lot. Open trellis work or similar features that can be seen through may extend above the wall. Street trees with canopies above pedestrian height may be included. Planting beds must be at least five feet wide.

Response: This temporary existing surface parking serves as an interim measure until phase 2 begins and a parking agreement will be in place for the use of these spaces. This parking space will include an evergreen hedge on both the west and east side of the parking lot.

- (B) Visually screen parking lots from abutting residential single zones and residential transition areas. Chain link fencing with slats is not an acceptable screen.

Response: The lot that provides the surface parking does not abut a residential single zone or residential transition area.

d. Refuse Enclosures

- ii. *Guideline.* Locate refuse storage to the rear of the site and away from major pedestrian walkways.

Response: The Refuse area will be located inside the parking garage, level P1 near the garage entry, hidden from view.

- iii. *Standard.* Refuse containment areas shall be placed in a building or enclosed in a structure that is of similar architectural character of the major structures on the site.

Response: See comment above.

e. Rooftop Screening

- ii. *Standard.* Screen mechanical equipment by extending the parapet walls or other roof forms that are integrated with the architecture of the building to a height that equals or exceeds the height of the mechanical equipment.

Response: The mechanical equipment will be located on the lower roof of the commercial space (BHA offices) between the residential building and the upper commercial roof. It will be screened from view through the use of metal louvered or perforated screening on the north façade which will be stepped back from the roof edge approximately 16 feet. Between the inconspicuous location, its height above grade and its distance from the roof edge, the screens should be relatively imperceptible.

f. Pedestrian Connectivity

- ii. *Guideline.* When a development encompasses a site greater than 350 feet wide or deep, incorporate pedestrian access through the site to connect to alleys, streets or neighboring properties.

Response: For the phase 1 development the total length of the development parcel is 376' from east to west. However, the west end of the building is roughly 20' from the westernmost property line and over 60' from the easternmost corner of the triangular lot, meaning the building actually occupies less than 300' of the block with open space at each end. Additionally, the proposed site plan provides a path through the corner of the triangular site reducing the distance between the Laurel sidewalk to

roughly 315'. A path through the building is available at the main entrance for use by building tenants. A design departure is requested.

For Phase 2 the full Podium will be constructed with an elevated landscaped courtyard. The eastern property line is roughly 414' and will require a design departure. This elevated courtyard will be available for use by all tenants and intermittent access will be provided to neighbors. For security and child safety this area will be secure, but tenants will have the ability to pass through the site from one end to the other.

For both phases, it is our assertion that due to the topography and shape of the site that difficult physical circumstances exist meriting a design departure.

- iii. *Guideline.* Provide walkways connecting access points within the site and from the site to the street. Locate walkways in active areas visible from adjoining spaces.

Response: This development provides walkways from the public sidewalk to building access points that are in active areas visible from the public right of way and tenant units above.

- iv. *Standard.* Pedestrian through-block connections shall not be less than five feet wide and be clearly defined by use of material, elevation (six inches raised) or landscaping borders.

Response: The through block connection at the northeast corner of the lot is greater than 5' and will continue the same material and pattern as the public sidewalks it connects.

g. Variations on Building Design

- ii. *Guideline.* When a development includes three or more buildings, the buildings within a development should not use identical footprints or elevations. Changes in textures and colors may be appropriate. While there may be some elements that provide a commonality to the entire development, variety shall be visually evident.

Response: The masterplan including both phases of construction will realize 3 separate multi-family buildings grouped over structured parking with a central courtyard. Each building offers a unique shape and height. When fully designed, the textures and colors of each building will vary while expressing commonality that is visually evident.

h. Site Lighting

- ii. *Guideline.* Low intensity lighting should be provided for entries, walkways, parking garage entrances, parking lots, alleys and refuse enclosures.

Response: Lighting provided as part of this development will meet all applicable code requirements and utilize low intensity as required.

- iii. *Guideline.* Parking lot lights should generally be no more than 18 feet in height and lower for pedestrian walkways.

Response: Surface parking lighting will be no more than 18' and located on the adjacent lot.

- iv. *Guideline.* Lighting should be directed away from the sky, dwellings and neighboring development. The use of LED or low energy use fixtures is strongly encouraged.

Response: Lighting provided will meet all applicable code requirements and use LED and low energy as much as possible.

- v. *Guideline.* Light fixtures should contribute to the overall design of the building and may be used to highlight special architectural features.

Response: Lighting provided will accent the building features while providing appropriately safe lighting levels.

2. Building Design

a. Massing and Articulation

- ii. *Guideline.* Buildings should convey a visually distinct base and top.

Response: The commercial façade (east elevation) provides a distinct roof with overhang while delicately touching the grade through the use of oversized contemporary pilaster type forms. The majority of the north residential façade is grounded by a row of townhouses and a brick faced podium level with fiber cement paneling above. The parapet roof ends at varying heights, but clearly expresses the buildings top.

- iii. *Guideline.* Roof parapets and eaves are encouraged to vary in height to avoid long, straight lines at the maximum building heights.

Response: The building parapet rooftop varies in height throughout the building perimeter.

- iv. *Guideline.* Buildings with facades longer than 50 feet should be divided into “modules” that are expressed three dimensionally throughout the building facade. Modules could vary in width.

Response: The building façades employ modulation breaking up flat planes creating a handsome and visually interesting building. However, the eastern portion of the south façade of the residential portion of the building contains a portion of the façade that extends 75’ without modulation. This portion of the façade will ultimately become part of the courtyard with phase 2 and not prominently visible from the street. The exterior wall of the parking structure extends 110’ without modulation, but this portion of wall will become internal once phase 2 is constructed. A design departure is requested.

vi. Standards

- (A) The maximum height within 15 feet of any street right-of-way is three stories and 40 feet. The minimum height within 15 feet of any street right-of-way is 18 feet. A maximum of 50 percent of wall length above the third story may encroach five feet into the setback (not including decks).

Response: The highest portion of the proposed building within 15’ of the right of way is less than 30’ above the adjacent grade. The lowest portion is 24’ above the adjacent grade.

- (B) Provide at least one setback for every three stories in similar fashion to subsection (B)(2)(a)(vi)(1) of this section on building elevations that front public spaces and historic buildings on the city, state or national historic registers. This provision shall apply regardless of whether the proposed development is built up to, or set back from, the common property line.

Response: This building does not front public spaces or historic buildings.

b. Ground Floor Details

- ii. *Guidelines.* Ground-floor, street-facing facades of commercial and mixed-use buildings shall incorporate elements such as lighting or hanging baskets, medallions, belt courses, plinths for columns, kick plates, projecting sills, tile work, pedestrian scale signs, planter boxes or other vegetation, or other elements that meet the intent.

Response: The ground floor façade of the commercial space shall employ lighting as required to provide visual interest at the pedestrian level.

c. Structured Parking

- ii. *Guidelines.* Structured parking levels that are adjacent to a public street or open space or a residential zone shall be screened or treated architecturally by window openings, landscaping designed to screen the facade, decorative meter grills, and/or other approved devices that meet the intent.

Response: The majority of the structured parking is hidden behind commercial and residential space. However, the structured parking will be visible along the south façade until phase 2 is constructed. Landscaping has been proposed as an interim measure to screen the façade until that time, see L101.

d. Building Material

- ii. *Guideline.* Design architectural features that are an integral part of the building and discourage features that appear to be “tacked on” or artificially thin.

Response: All architectural features have been designed to be an integral part of the building.

- iii. *Guideline.* Material selection should exhibit high quality, particularly at street level. Examples include stone, brick, tile, concrete, metal, or transparent glass. Avoid residential lap siding on the ground floor of commercial or mixed-use buildings.

Response: The proposed building intends to utilize brick, metal, glass and fiber cement panel siding.

- iv. *Guideline.* Encourage the use of recessed windows that create shadow lines.

Response: Recessed windows are proposed on the commercial space (BHA offices) on both Otis St. & Samish Way. Additionally, 4 levels of recessed storefront will be utilized at the main residential entry on Otis St as well as a portion of the windows along the south facing façade.

e. Transparency

- ii. *Standard.* A minimum of 60 percent of the building wall between two feet and seven feet above the sidewalk and facing a street shall be transparent or lightly tinted glazing. Windows into parking garage space shall not qualify. If windows are not appropriate, glass display cases, decorative art (for example, murals or relief sculpture), significant architectural detailing or wall-covering landscaping may be used.

Response: The proposed commercial façade along Samish and Otis contains at least 60% glazing between 2 and 7 feet, with the exception of the garage entry.

f. Weather Protection

- ii. *Guideline.* Weather protection should be located between eight and 12 feet above the walkway but a higher placement may be considered if the width of the protection is increased.

Response: Weather protection will be provided along Samish Way, where the proposed building extends to the property line. A roof overhang will extend at least 6'-6" over the sidewalk for 50' at a height of 30' above grade. A second roof overhang will extend 34" over the sidewalk for 64' at a height of 24'. Additional overhang exists on the Otis St façade, but with setbacks the building is held back from the pedestrian sidewalk with a landscaped separation making an awning of no functional effect. Overall, the awnings are designed to be essential elements to the building's architectural composition to establish an attractive anchor at this prominent intersection while providing weather protection where most necessary. A design departure is requested.

- iii. *Standard.* Where commercial uses are located at ground floor, buildings shall provide pedestrian weather protection covering at least a four-foot width of sidewalk along at least 75 percent of the street level frontage. Minimum adjustments needed to accommodate trees or other overhead objects may be allowed. Materials shall be limited to metal, glass, Plexiglas or equivalent "hard" durable materials.

Response: While the proposed building does not meet the 75% threshold, the current pedestrian traffic along Samish Way is limited. Additionally, the greatest cover is offered where the majority of the pedestrian traffic will occur, at the main entry to the commercial space. Street trees also impede the proposed building's ability to meet this standard. We believe we have met the intent of the standard and the proposed design will not have any substantial detrimental effect on nearby properties and the city or neighborhood. A design departure is requested.

g. Blank Walls

- ii. *Standard.* A blank wall is a wall or a portion of wall that is either (A) at least 400 square feet in area with a length and width of at least 10 feet that does not include a window, door, building modulation or other architectural feature or (B) at the ground floor and over six feet in height with a horizontal length greater than 15 feet that does not include a window, pedestrian entry door, building modulation or other architectural feature. Any blank wall within 50 feet of, and visible from, a public street, public park or trail, residential zone shall be treated with at least one of the following:

- (A) Artwork, such as bas relief sculpture, mural or similar feature.
- (B) A landscaping bed containing trees, shrubs and/or vines on a trellis that will cover at least 60 percent of the wall within three years. Irrigation shall be provided unless the bed is at least five feet in width, open to the sky and drought resistant plants are used.
- (C) Architectural detailing incorporating trims, textures, reveals, contrasting materials, or other special detailing that provides visual interest.

Response: All blank walls have been addressed by response to DRB comments.

i. Projections into the Public Right-of-Way

- ii. *Standards.* No portion of a building may extend into the street right-of-way except:

- (A) Decks and balconies shall not project more than two feet into the right-of-way unless a fixed awning covers the entire space directly below the deck or balcony at the ground floor level. A deck or balcony may not in any case project more than four feet into the right-of-way.

Response: No decks or balconies extend into the right-of-way.

- (B) Bay windows and similar architecture features with a horizontal width of not greater than 12 feet that project no more than four feet into the right-of-way from the face of the exterior wall, provided they are separated from like features by at least an equal width may protrude into the right-of-way.

Response: No bay windows exist in the proposed project.

- (C) Columns, cornices, trellises, eaves and similar minor and/or decorative features; provided, that arcades shall not be located in the street right-of-way.

Response: No architectural features as listed extend into the public right-of-way.

- (D) Steps, stoops and similar ground level features, provided there is a minimum 12 feet of horizontal clearance to the street curb edge.

Response: No steps or stoops extend into the public right-of-way.

- (E) Awnings, marquees and signs, subject to compliance with other codes, provided no ground-mounted support structures for these features shall be located in the right-of-way.

Response: Signage will abide by these standards.

j. Residential Design

ii. Standards.

- (A) Buildings containing residential uses shall have at least one covered front residential entryway facing a public right-of-way and accessed directly from the adjoining sidewalk.

Response: One covered front residential entry is provided via sidewalk on the north façade of the residential portion of the building.

- (B) Open exterior entry/exit balconies that face a right-of-way are prohibited.

Response: The proposed project complies.

- (C) Residential units built within 10 feet measured horizontally of an adjoining right-of-way or public space (property line) shall be constructed so that the finished floor elevation is at least 30 inches above the adjoining sidewalk.

Response: No residential units exist within 10' of an adjoining right of way.

- (D) Ground floor residential units fronting on a street shall have a private main entry to the sidewalk consisting of a stoop or porch. Patio access doors shall not be considered a main entry.

Response: The topography of this site transitions over 20' from the west end of the building to the east, creating some difficulty in defining which residential units are actually at the "ground floor." The proposed building includes 5 townhouse style units

which have a private stoop and entry complying with this standard. Two additional 1 bedroom units are provided along Otis at the podium level, but the proposed grade along that facade quickly lowers below their floor level. An additional 2 units exist at the west end of the proposed building, the furthest unit to the south is above the exterior grade. The unit at the northwest corner of the building is the only ground floor unit that is truly at grade. However, providing access to this unit from grade would isolate its access at a featured landscape element on the intersection of Laurel & Otis that would interrupt the semi-public greenspace that opens to the podium access. A design departure is requested due to difficult physical circumstances.

- (E) When private interior courtyards interface the street edge, use a landscape hedge, ornate fencing, architectural walls, or a combination of the above to carry the wall line at the street edge and define the private space.

Response: The proposed building has 5 private yards associate with the townhome units. These yards employ landscaping, a concrete retaining wall and fencing.

20.37.130 Samish Way urban village - Development

C. Design Standards

1. Commercial Street Frontage

- b. *Standards.* Along Samish Way, between Bill McDonald Parkway and E. Maple Street, ground floor commercial space (including retail, service, office, government, or similar nonresidential uses) shall be provided along the full building street front for a depth of at least 20 feet measured from the front face of the building. Lobbies for residential uses and hotels and parking garage entries shall qualify as ground floor commercial; however, hotel/motel guest rooms, dwelling units and structured parking shall not qualify.

Response: The proposed building includes commercial space along the entire length of its ground floor building façade along Samish Way. The depth of this space is greater than 20'.

2. Yards

- b. *Standard.* A building shall be set back a minimum of 15 feet from a property line abutting or across a right-of-way from a residential single zone or a residential transition area. (See Figure 20.37.130.)

Response: The Otis street façade is across the right-of-way from a residential transition zone. This façade is set back greater than 15' from the property line in compliance with this standard.

3. Height

- b. *Standard.* The maximum height within 30 feet of a property line abutting or adjacent to a residential single zone or a residential transition area shall be two stories and 30 feet. (See Figure 20.37.130.)

Response: As mentioned above the Otis street façade is across the right-of-way from a residential transition zone. This façade steps back in compliance with this standard so that the portions of the building exceeding 30' are at least 30' from the property line.

Design Guidance Provided by DRB

1. Given the budget for affordable housing, use higher quality siding panels as the project would allow. Otherwise concern was expressed about the weathered look and maintenance of lower quality materials over time in the Pacific NW climate.

Response: As much as is possible, the development of this project will include higher quality exterior finishes. These products tend to be less maintenance intensive and are valuable to the building owner if economically feasible.

2. Granting of a design departure from BMC [20.25.070](#)(B)(2)(g) for the blank wall adjacent to Samish Way (Figure 1.2 in the staff report) is recommended given its temporary nature.

Response: The proposed blank wall has been eliminated in favor of glazing at the SW wall location of the BHA offices.

3. Stair towers:
 - a. Treatment of blank walls generally not as much an issue since so much else going on with the building.

Response: OK.

- b. The panel siding seams should provide a measure of depth and texture.

Response: Both stair towers will have an exterior finish of fiber cement panel siding. This material is applied over a rainscreen with open seams approximately every 4' to 8' both horizontally and vertically.

- c. Consider adding a ribbon of glass visible from Samish Way on the smaller tower similar to the ribbon on the larger tower.

Response: A ribbon window has been added to the south side of the east stair tower, which is visible from Samish Way.

4. Add kitchen windows to the elevation in Figure 1.4 in the staff report.

Response: Kitchen windows have been added to this elevation.

5. Add bathroom windows to the elevation in Figure 1.1 of the staff report (misabeled as kitchen windows in Figure 1.1)

Response: Bathroom windows have been added to this elevation.

6. Take a look at the window and floor plan options for the corner units on Figure 1.1 to with intent to provide more visual emphasis to the building module corner and increase livability of the corner units.

Response: The windows and corresponding floor plans need to remain as originally presented. Structural and plumbing considerations don't allow an easy reconfiguration of these spaces. Additionally, the window rhythm created along the North façade would be interrupted.

Design Guidance Provided by Staff

7. Granting of design departures is recommended as follows:
- a. From BMC 20.25.070(B)(1)(b) for interim use of existing open parking adjacent to Laurel Street and N. Samish Way.

Response: OK.

- b. From BMC 20.25.070(B)(1)(f)(2) for the requirement to provide a pedestrian connection through the site.

Response: OK.

- c. From BMC 20.25.070(B)(2)(f) to reduce the width and increase the height of the awnings associated with the commercial portion of the building.

Response: OK.

8. Strengthen the tops of the building's book-end masonry modules (one or both) consistent with BMC 20.25.070(B)(2)(a)(ii). This could potentially be accomplished with a change in brick color, reveal, and/or pattern, adding a cornice, etc.

Response: Both masonry building ends have been modified to include a cornice at their respective tops.



EXHIBIT B

Permit Center

210 Lottie Street, Bellingham, WA 98225

Phone: (360) 778-8300 Fax: (360) 778-8301 TTY: (360) 778-8382

Email: permits@cob.org Web: www.cob.org/permits

Land Use Application

Check all permits you are applying for in the boxes provided. Submit this application form, the applicable materials listed in the corresponding permit application packet(s) and application fee payment.

<input type="checkbox"/> Accessory Dwelling Unit <input type="checkbox"/> Binding Site Plan <input type="checkbox"/> Clearing Permit <input type="checkbox"/> Conditional Use Permit <input type="checkbox"/> Critical Area Permit <input type="checkbox"/> Minor Critical Area Permit <input type="checkbox"/> Design Review <input type="checkbox"/> Grading Permit <input type="checkbox"/> Home Occupation <input type="checkbox"/> Institutional <input type="checkbox"/> Interpretation <input type="checkbox"/> Landmark – Historic Certificate of Alteration <input type="checkbox"/> Legal Lot Determination <input type="checkbox"/> Nonconforming Use Certificate	<input checked="" type="checkbox"/> Parking Adjustment Application <input type="checkbox"/> Planned Development <input type="checkbox"/> Rezone <input type="checkbox"/> SEPA <input type="checkbox"/> Shoreline Permit <input type="checkbox"/> Shoreline Exemption <input type="checkbox"/> Subdivision-Short Plat/Lot Line Adjustment <input type="checkbox"/> Subdivision-Preliminary Plat <input type="checkbox"/> Subdivision-Final Plat <input type="checkbox"/> Variance <input type="checkbox"/> Wireless Communication <input type="checkbox"/> Zoning Compliance Letter <input type="checkbox"/> Other: _____	Office Use Only Date Rcvd: _____ Case #: _____ Process Type: _____ Neighborhood: _____ Area Number: _____ Zone: _____ Pre-App. Meeting: _____ Concurrency: _____
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Project Information

Project Address 301 & 315 N. Samish Way, Bellingham, WA Zip Code 98225

Tax Assessor Parcel Number (s) 380331425243 & 380331421560000

Project Description Phased development of the former Aloha! Motel site into a mixed use multi family projects
consistent with the City's vision for the Samish Way Urban Village subarea plan.

Applicant / Agent

☒ Primary Contact for Applicant

Name RMC Architects, Robert Wright

Mailing Address 1223 Railroad Ave.

City Bellingham State WA Zip Code 98225

Phone 360.676.7733 Email robert.w@rmcarchitects.com

Owner (s)

☐ Applicant

☐ Primary Contact for Applicant

Name Bellingham Housing Authority, Ken Van Winkle

Mailing Address PO Box 9701

City Bellingham State WA Zip Code 98227-9701

Phone 360.676.6887 Email kvanwinkle@bwcha.org

Property Owner(s)

I am the owner of the property described above or am authorized by the owner to sign and submit this application. I grant permission for the City staff and agents to enter onto the subject property at any reasonable time to consider the merits of the application and post public notice. I certify under penalty of perjury of the laws of the State of Washington that the information on this application and all information submitted herewith is true, complete and correct.

I also acknowledge that by signing this application I am the responsible party to receive all correspondence from the City regarding this project including, but not limited to, expiration notifications. If I, at any point during the review or inspection process, am no longer the Applicant for this project, it is my responsibility to update this information with the City in writing in a timely manner.

Signature by Owner/Applicant/Agent  Date 05/13/2019

City and State where this application is signed: Bellingham WA
City State



Permit Center

210 Lottie Street, Bellingham, WA 98225
Phone: (360) 778-8300 Fax: (360) 778-8301 TTY: (360) 778-8382
Email: permits@cob.org Web: www.cob.org/permits

PARKING ADJUSTMENT APPLICATION

(PLEASE TYPE OR PRINT CLEARLY IN BLUE OR BLACK INK)

Application Requirements:

- ☒ A completed Land Use Application form
- ☒ All of the materials and information required by this form
- ☒ Application fee payment

The below listed property owner(s) hereby make a request for:

- ☐ Parking waiver under BMC Section 20.12.010 ☐ (A)(3) ☐ (A)(4)
- ☒ Parking reduction under:
 - ☒ BMC Section 20.12.010(A)(5)(a-c) ☐ BMC Section 20.12.010(A)(5)(d)
 - ☐ BMC Section 20.35.075(D) ☒ BMC Section 20.37.150 (E) for Samish Way Urban Village
- ☐ Joint/shared parking among uses on the **same site** under BMC Section 20.12.010(A)(6)
- ☐ Joint/shared parking among uses on **different sites** under BMC Section 20.12.010(A)(6)

Property owner(s) (List all persons with an ownership interest in the property if different than applicant.):

Name: _____ Phone: _____ Fax: _____

Address: _____

Signature: _____

Name: _____ Phone: _____ Fax: _____

Address: _____

Signature: _____

Property Owner(s)

I am the owner of the property described above or am authorized by the owner to sign and submit this application. I grant permission for the City staff and agents to enter onto the subject property at any reasonable time to consider the merits of the application and post public notice. I certify under penalty of perjury of the laws of the State of Washington that the information on this application and all information submitted herewith is true, complete and correct.

Signature by Owner/Applicant/Agent *[Signature]* *agent of the owner*, Date 5/13/19

City and State where this application is signed: Bellingham, WA

City

State

Street Address(s) and parcel number(s) of subject property requiring parking: **(required for complete application):**

Address: 301 & 315 N. Samish Way, Bellingham, WA 98225

Tax Assessor's Parcel Number(s) 380331421560000

Legal description of property requiring parking (attach if necessary): ELDRIDG-BARTLETT'S ADD TO SEHOME LOTS 10

THRU 16 BLK 26-TPG WI VAC SE 10 FT OTIS ST ABTG- TOG WI VAC NE 10 FT LAUREL ST ABTG LOT 16-TOG WI VAC NW 1/2 ALLEY ABTG LOTS 13 THRU 16-LESS RD

Description of the proposed project requiring parking: 69 unit mixed-use multi-family building with 2 levels of structured parking beneath 4 levels of residential. 11,000 SF of commercial space fronting on Samish Way.

Parking Spaces:

Number required for proposed project under the standard parking requirements of BMC 20.12 (attach calculations): 95 Spaces, see sheet G0001

Number of parking spaces to be provided on site: 85 + 4 motorcycle spaces

Parking Waiver / Reduction Requests (Complete #1 – 3)

1. Number of required parking spaces requested to be waived: 0
2. Number of required parking spaces requested to be reduced: 10
3. Number of street parking stalls available within a foot walking distance: 23 off site street parking spots immediately adjacent the site

Joint/Shared Parking Among Uses on Different Sites (Complete #4 – 7)

4. Street Address and parcel number(s) of property providing joint parking (must be within 1,000 feet of subject project):

Address: _____

Tax Assessor's Parcel Number(s) _____

Legal description of property providing joint parking (attach if necessary):

5. Number of parking spaces on the site listed in #4 to be used exclusively by the subject project (private off-site parking): _____
6. Number of parking spaces on the site listed in #4 to be shared with the subject property due to different hours of parking demand: _____
7. Describe or attach draft legal documents proposed to regulate the joint parking. Indicate whether it is an easement or agreement, the duration and the terms for cancellation. A permanent parking easement may be required as a condition of approval.

SUBMITTAL REQUIREMENTS:

The following submittals are required for all parking adjustment requests. (Site and floor plan submittals may be waived if these plans have been submitted with a concurrent building or land use permit application.)

1. Site and Parking Plan (2 sets)

- ☒ Scale, north arrow and date drawn. Scale shall be standard architectural or engineering. The scale must allow clear depiction of all required information, typically between 1" = 10' and 1" = 40'. (Planned development: between 1" = 20' and 1" = 100').
- ☒ All lot lines and site boundary dimensions.
- ☒ Location, dimensions, gross floor area and use of existing and proposed structures. Indicate all structures to be demolished. Clearly label and distinguish proposed from existing structures.
- ☒ Distances between structures and property lines.
- ☒ Location and design of all paving.
- ☒ Walkways and bicycle paths.
- ☒ Existing and proposed easements.
- ☒ Distances between proposed buildings, parking areas and property lines.
- ☒ Location, dimension and number of parking spaces (including accessible spaces), bicycle parking, drop-off areas and driveway access.
- ☒ All significant man-made or natural features (retaining walls, ponds, woodlands, streams, etc.)
- ☒ Abutting streets and sidewalks; note any on-street parking areas.
- ☒ Any other feature necessary to explain the request or address the criteria for approval.

2. Floor Plans

- ☒ Scaled floor plans for all existing and proposed buildings with uses labeled.

3. Parking Studies and Documents

All Applications

- ☒ Parking calculations for all facilities associated with the parking waiver, parking reduction or shared parking, based on BMC 20.12 parking requirements.

Parking Waiver

- ☐ Provide an analysis of the effect of the proposed parking waiver on street parking demand consistent with BMC 20.10.010(A)(3) or (4). The Director may require specific information or a professional analysis, depending on the location and amount of spaces requested to be waived.

Parking Reduction

- ☒ Provide an analysis of the effect of the proposed parking reduction consistent with BMC 20.10.010(A)(5)(a-c) or the corresponding urban village per BMC 20.35 or 20.37. The Director may require specific information or a professional analysis, depending on the location and amount of spaces requested to be reduced.

-OR-

- ☐ The applicant will pay into a City adopted fee-in-lieu-of-parking program at the prevailing rate established by the program, consistent with BMC 20.10.010(A)(5)(d).

Shared Parking Due to Different Hours of Parking Demand

- ☐ Provide a parking demand analysis prepared by a professional with expertise in preparing parking and traffic analysis pursuant to BMC 20.10.010(A)(6). Professional preparation may be waived by the Director based on the scope of the request. The analysis shall include peak hour data and use commonly accepted methodology such as the Urban Land Institute Shared Parking study.

SAMISH WAY REDEVELOPMENT (ALOHA!)
PARKING ADJUSTMENT APPLICATION
REDUCTION ANALYSIS

The Samish Way Redevelopment project is seeking a reduction of 23 parking spaces (24.2%) for its proposed development. As calculated on sheet G001, the required parking for this project is 95 parking spaces. We have proposed 72 parking spaces, plus an additional 13 temporary parking spaces in a temporary lot, 4 motorcycle parking spaces and an available 23 off-site parking spaces located at the perimeter of the property. In addition to the vehicle parking, 28 bicycle parking spaces have been provided, which nearly doubles the 15 spaces required per BMC 20.37.150 (F).

To obtain this waiver, we would like to employ BMC 20.37.150 (E), which allows the planning director to administratively reduce the parking requirement up to 30% on the basis of this development serving as affordable housing. Additionally, BMC 201.12.010 (A) (5) (b & c) allows the director to reduce the parking requirement up to 25% if the project is within $\frac{1}{4}$ mile of a WTA go route or if enhanced bicycle and pedestrian facilities are offered. This development is located on the WTA Blue Line Go route, with a bus stop located directly across Samish Way. Bicycle parking nearly doubles the required number of spaces with both outdoor short-term parking and more secure longer term covered parking on both parking levels monitored by security cameras. Pedestrians will now enjoy a sidewalk along Otis Street where there isn't one currently and a widened sidewalk along Samish Way. Anecdotally, the office space required parking may benefit from utilization at a different time than the required residential parking.

In summation, this project satisfies multiple code mechanisms to reduce the required parking at the discretion of the director. Thank you for your consideration.

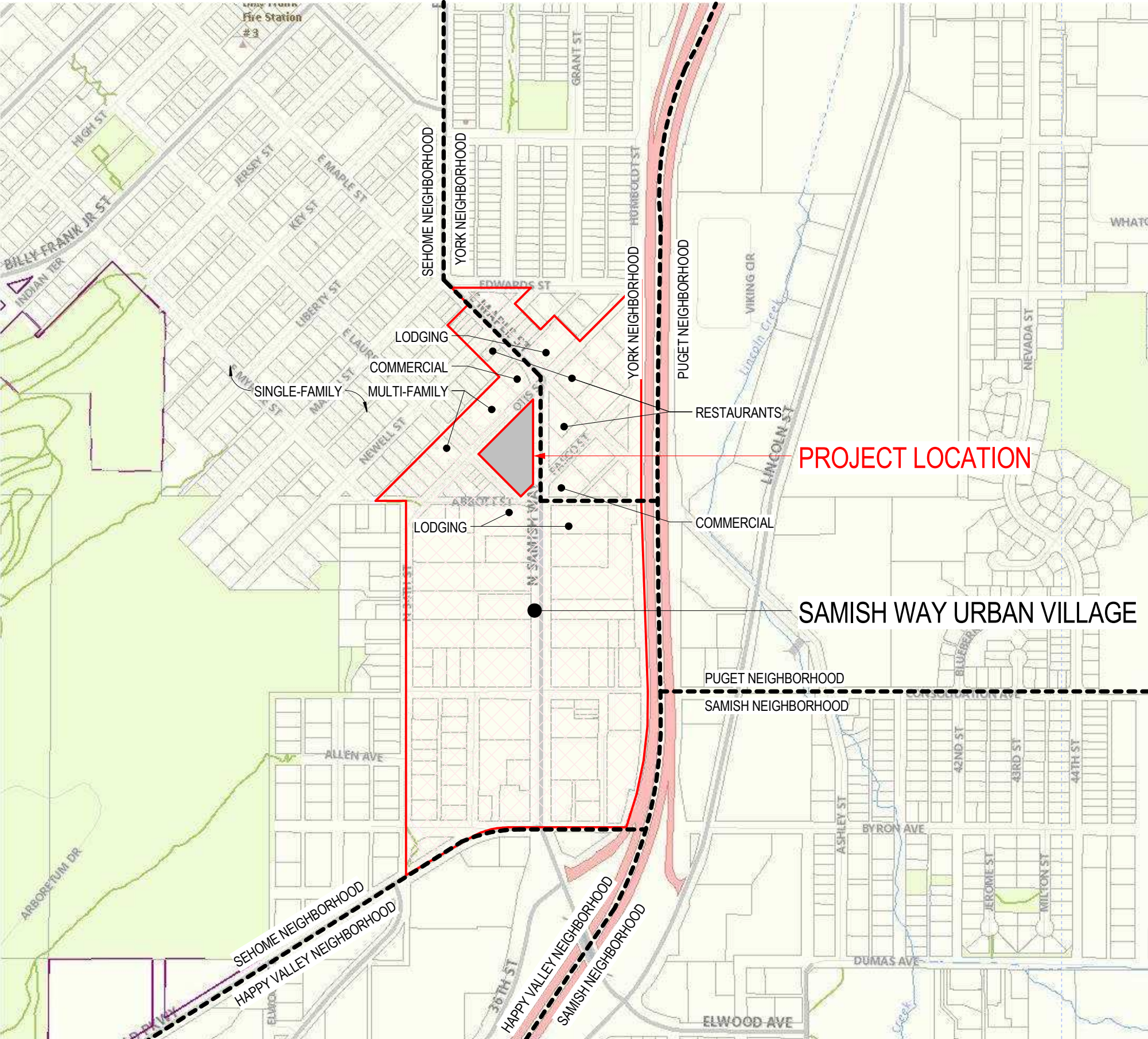


RMC Architects | Stephanie Bower, Architectural Illustration

Rendering of Proposed Exterior

NO SCALE
NOTE: THIS RENDERING IS FOR ILLUSTRATIVE PURPOSES ONLY. NOT TO BE USED FOR DESIGN, PERMITTING, BIDDING, OR CONSTRUCTION.

Vicinity Map



Project Team

- CLIENT: BELLINGHAM HOUSING AUTHORITY
PO BOX 9701
BELLINGHAM, WA 98227-9701
T 360 676 5885
ATTENTION: KEN VAN WINKLE
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robert.w@rmcarchitects.com
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PO BOX 30923
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berni@king-works.com
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ATTENTION: JOHN GREENLAW
john@greenbush.com
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208 THIRD STREET
LYNDEN, WA 98264
T 360 554 4757
ATTENTION: STEVE TEVELDE
stavevelde@k-engineers.com

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- G001 COVER SHEET
- ARCHITECTURAL
- A101 SITE PLAN
- A200 P1 LEVEL
- A201 P2 LEVEL
- A203 PODIUM LEVEL 1 RESIDENTIAL PLAN
- A204 LEVEL 2 RESIDENTIAL PLAN
- A205 LEVELS 3 & 4 RESIDENTIAL PLAN
- A206 ROOF PLAN
- A301 BUILDING ELEVATIONS
- A302 BUILDING ELEVATIONS
- A303 COLOR PERSPECTIVE
- CIVIL
- C101 COVER
- C102 EXISTING CONDITIONS
- C103 OTIS STREET GRADING & STORMWATER PLAN
- C104 OTIS STREET WATER & SANITARY SEWER PLAN
- C105 GRADING & STORMWATER PLAN
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- C107 TEMPORARY EROSION AND SEDIMENT CONTROL
- C108 DETAILS
- C109 DETAILS
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- C113 DETAILS
- C114 P1 LEVEL GRADING PLAN
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- L101 LANDSCAPE REFERENCE PLAN
- L102 LANDSCAPE PLANT SLAVAGE PLAN
- L103 LANDSCAPE DETAILS
- L104 LANDSCAPE PLANTING PLAN
- L105 PLANTING DETAILS
- L106 GREEN FACTOR
- L201 IRRIGATION PLAN
- L202 IRRIGATION DETAILS
- L203 IRRIGATION DETAILS

Project Information

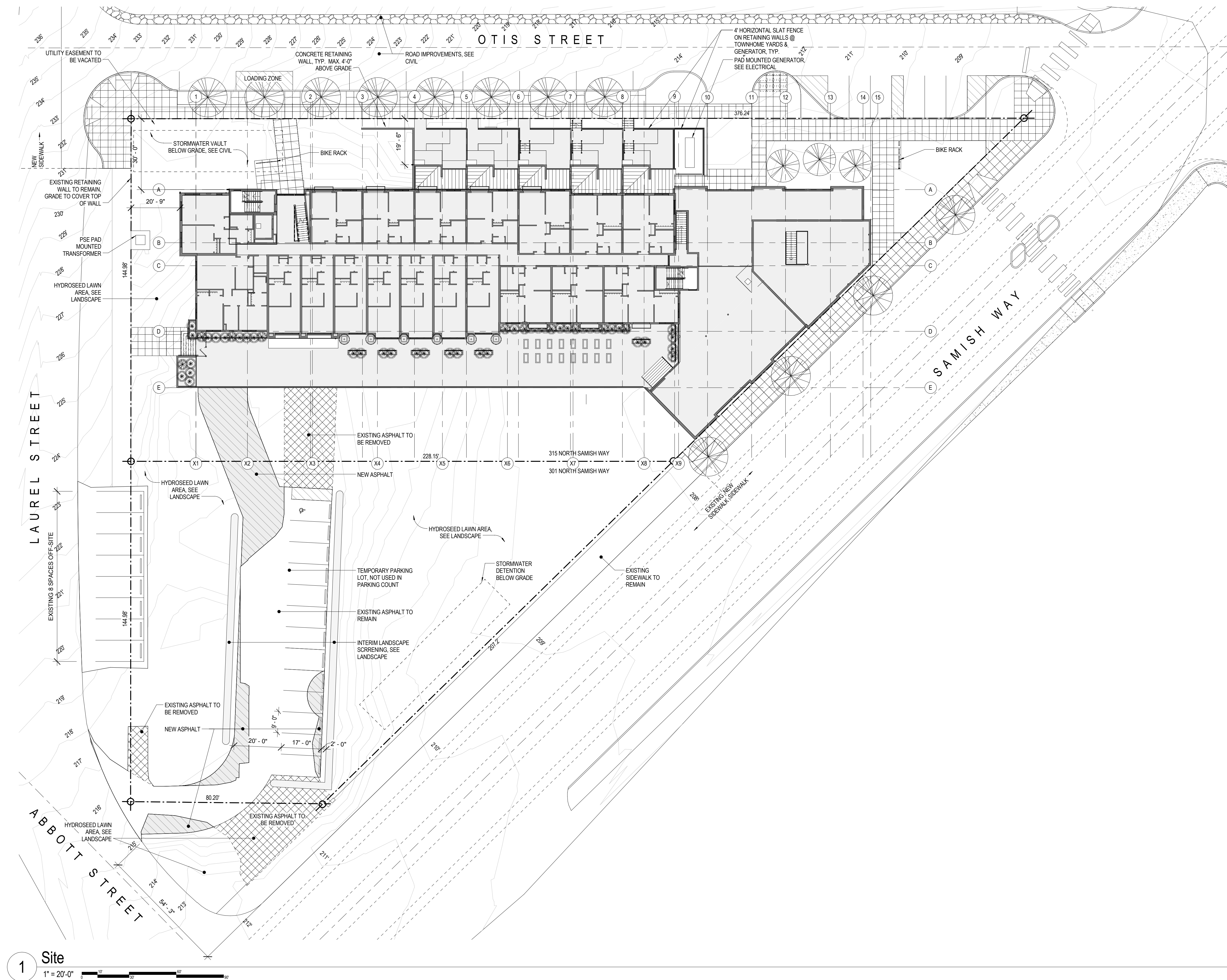
PROJECT DATA																																								
SITE ADDRESS:	301 & 315 NORTH SAMISH WAY BELLINGHAM, WA 98225																																							
PARCEL NUMBER:	380331421560000 & 3803314252430000																																							
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BUILDING CODE REQUIREMENTS																																								
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	*NOTE: ALL CODES ARE SUBSEQUENTLY MODIFIED BY WASHINGTON ADMINISTRATIVE CODE (WAC) AMENDMENTS																																							
HEIGHT LIMIT:	55' FROM AVERAGE GRADE PLANE																																							
SETBACKS:	SEE BMC 20.37.330 C																																							
OCCUPANCY CLASS:	S-2 PARKING B COMMERCIAL OFFICE R-2 RESIDENTIAL																																							
CONSTRUCTION TYPE:	TYPE I-A (PODIUM) TYPE V-A (RESIDENTIAL)																																							
	PER IBC 510.2, A THREE HOUR HORIZONTAL BUILDING SEPARATION IS REQUIRED BETWEEN TYPE I-A AND TYPE V-A CONSTRUCTION																																							
REG'D LAND USE APPLICATIONS:	PRE-APPLICATION CONFERENCE APPLICATION DESIGN GUIDANCE MEETING APPLICATION SEPA PARKING ADJUSTMENT APPLICATION PUBLIC FACILITIES																																							
FIRE PROTECTION:	NFPA-13 FIRE SPRINKLER SYSTEM																																							
SITE AREA:	43,755 SF																																							
LOT COVERAGE:	25,044 SF (57.2%)																																							
OPEN SPACE:	18,711 SF (42.8%)																																							
AREA SUMMARY:	<table><tr><td>PARKING</td><td>COMMERCIAL</td><td>RESIDENTIAL</td><td>TOTAL</td></tr><tr><td>P1 LEVEL 14,784 SF</td><td>4,265 SF</td><td>3,810 SF</td><td>19,049 SF</td></tr><tr><td>P2 LEVEL 11,834 SF</td><td>6,800 SF</td><td>12,155 SF</td><td>22,444 SF</td></tr><tr><td>RESIDENTIAL PODIUM</td><td></td><td>12,155 SF</td><td>12,155 SF</td></tr><tr><td>RESIDENTIAL R2</td><td></td><td>12,240 SF</td><td>12,240 SF</td></tr><tr><td>RESIDENTIAL R3</td><td></td><td>12,240 SF</td><td>12,240 SF</td></tr><tr><td>RESIDENTIAL R4</td><td></td><td>9,192 SF</td><td>9,192 SF</td></tr><tr><td>TOTAL</td><td>26,618 SF</td><td>11,065 SF</td><td>49,637 SF</td></tr><tr><td></td><td></td><td></td><td>87,320 SF</td></tr></table>				PARKING	COMMERCIAL	RESIDENTIAL	TOTAL	P1 LEVEL 14,784 SF	4,265 SF	3,810 SF	19,049 SF	P2 LEVEL 11,834 SF	6,800 SF	12,155 SF	22,444 SF	RESIDENTIAL PODIUM		12,155 SF	12,155 SF	RESIDENTIAL R2		12,240 SF	12,240 SF	RESIDENTIAL R3		12,240 SF	12,240 SF	RESIDENTIAL R4		9,192 SF	9,192 SF	TOTAL	26,618 SF	11,065 SF	49,637 SF				87,320 SF
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			+ 4 MOTORCYCLE SPACES + 13 ONSITE TEMPORARY SPACES																																					
REQUIRED PARKING:	<table><tr><td>BHA OFFICE</td><td>11,065 SF</td><td>1/500 SF</td><td>23 SPACES</td></tr><tr><td>STUDIO</td><td>26 UNITS</td><td>1/UNIT</td><td>26 SPACES</td></tr><tr><td>ONE BEDROOM</td><td>30 UNITS</td><td>1/UNIT</td><td>30 SPACES</td></tr><tr><td>TWO BEDROOM</td><td>8 UNITS</td><td>1/UNIT</td><td>8 SPACES</td></tr><tr><td>THREE BEDROOM</td><td>5 UNITS</td><td>1.5/UNIT</td><td>8 SPACES</td></tr><tr><td></td><td>69 UNITS</td><td></td><td>96 SPACES*</td></tr></table>				BHA OFFICE	11,065 SF	1/500 SF	23 SPACES	STUDIO	26 UNITS	1/UNIT	26 SPACES	ONE BEDROOM	30 UNITS	1/UNIT	30 SPACES	TWO BEDROOM	8 UNITS	1/UNIT	8 SPACES	THREE BEDROOM	5 UNITS	1.5/UNIT	8 SPACES		69 UNITS		96 SPACES*												
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	*AFFORDABLE AND SENIOR HOUSING IS ALLOWED A PARKING REDUCTION VIA A PLANNING DIRECTOR WAIVER																																							
BICYCLE PARKING:	15% OF 96 REQUIRED VEHICLE PARKING SPACES (PER BMC 20.37.150 F) = 15 BICYCLE SPACES REQUIRED 28 BICYCLE SPACES PROVIDED																																							
UNIT SUMMARY:	<table><tr><td>STUDIO</td><td>26 UNITS</td><td>38%</td></tr><tr><td>ONE BEDROOM</td><td>30 UNITS</td><td>43%</td></tr><tr><td>TWO BEDROOM</td><td>8 UNITS</td><td>12%</td></tr><tr><td>THREE BEDROOM</td><td>5 UNITS</td><td>7%</td></tr><tr><td></td><td>69 UNITS</td><td></td></tr></table>				STUDIO	26 UNITS	38%	ONE BEDROOM	30 UNITS	43%	TWO BEDROOM	8 UNITS	12%	THREE BEDROOM	5 UNITS	7%		69 UNITS																						
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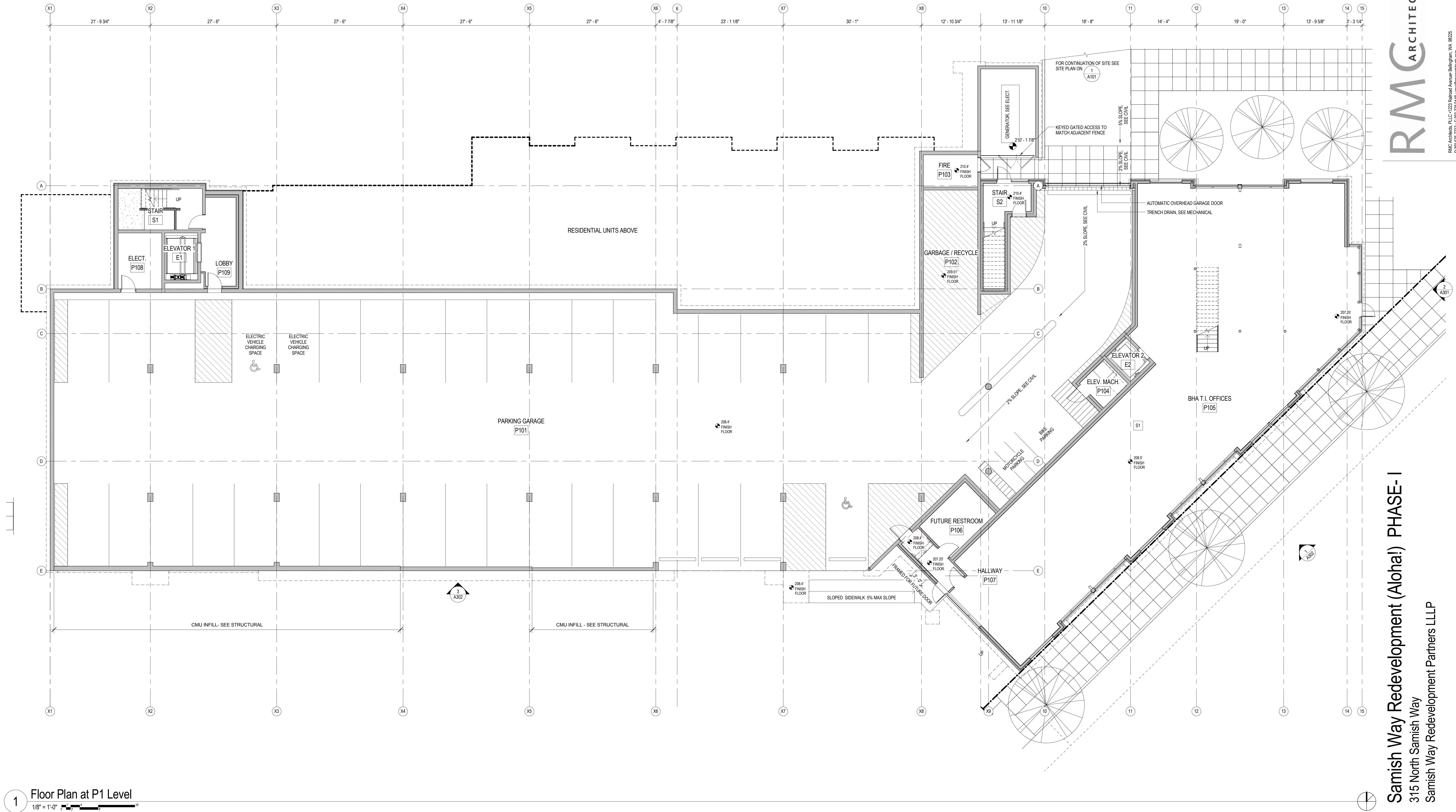
Samish Way Redevelopment (Aloha!) PHASE-I
315 North Samish Way
Samish Way Redevelopment Partners LLLP

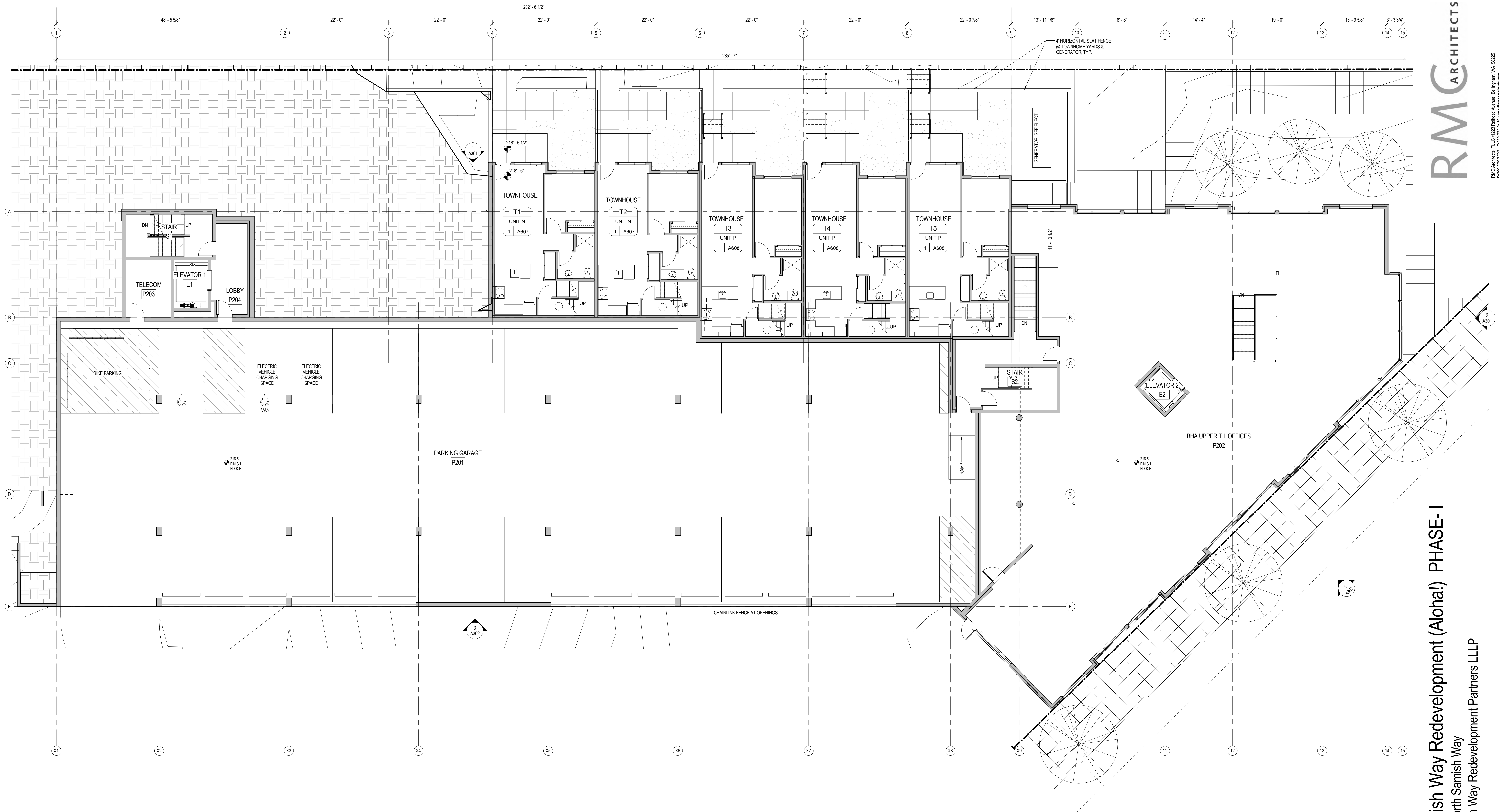
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File No: 1571.01 Phase I-BHA_DRB
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SITE PLAN

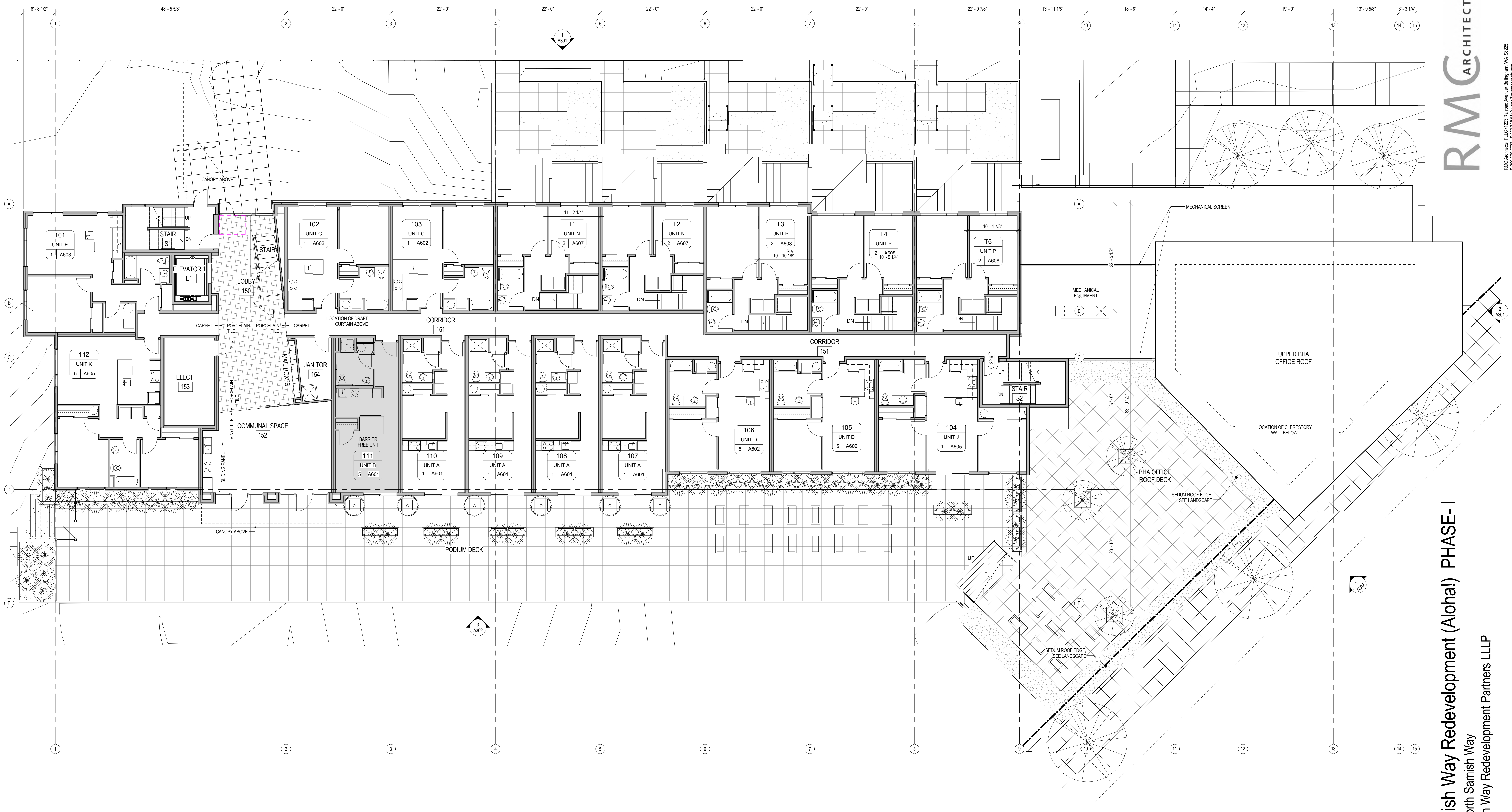
A101







1 Floor Plan at P2 Level
1/8" = 1'-0"



1 Podium Level 1 Residential Plan
1/8" = 1'-0"

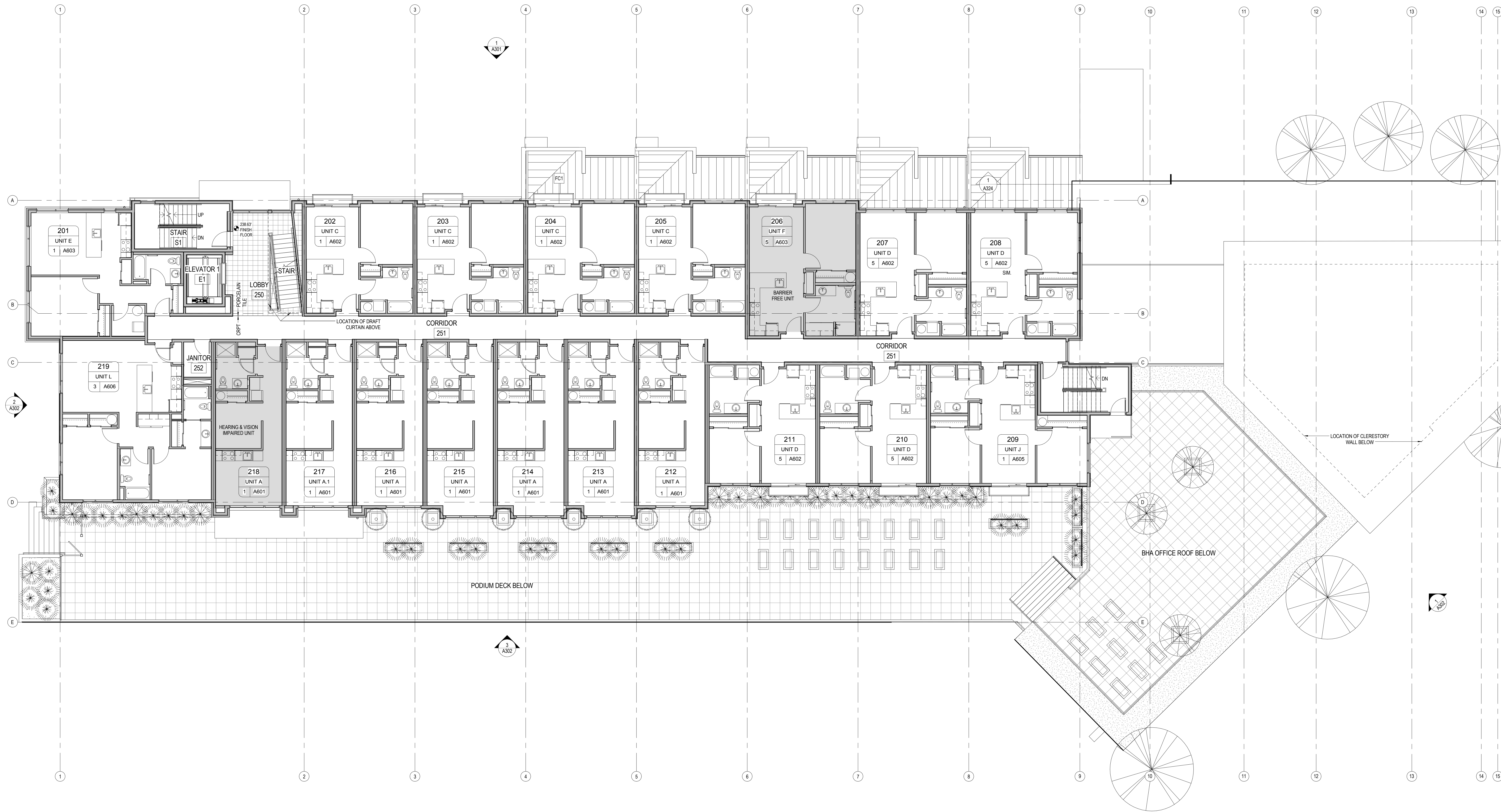
Samish Way Redevelopment (Aloha!) PHASE-I
315 North Samish Way
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PODIUM LEVEL 1
RESIDENTIAL
PLAN

A203



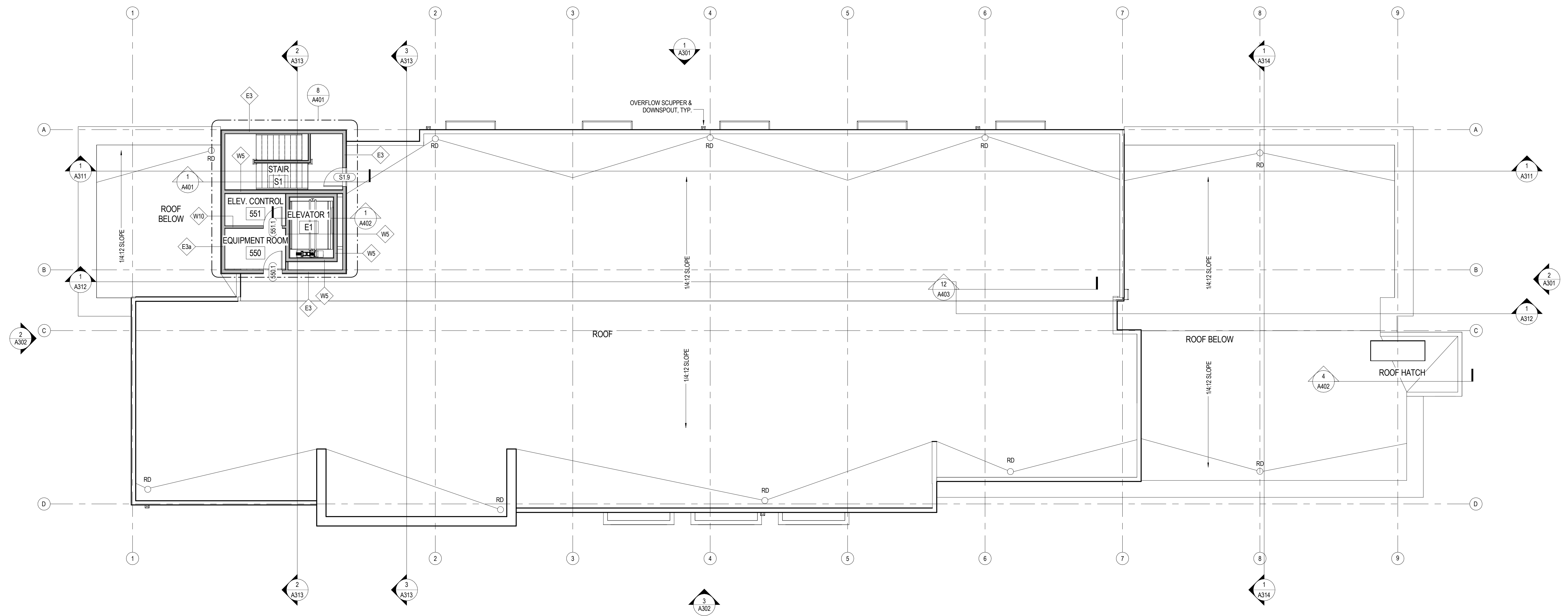
1 Level 2 Residential Plan
1/8" = 1'-0"



1 Level 3 Residential Plan
1/8" = 1'-0"



2 Level 4 Residential Plan
1/8" = 1'-0"



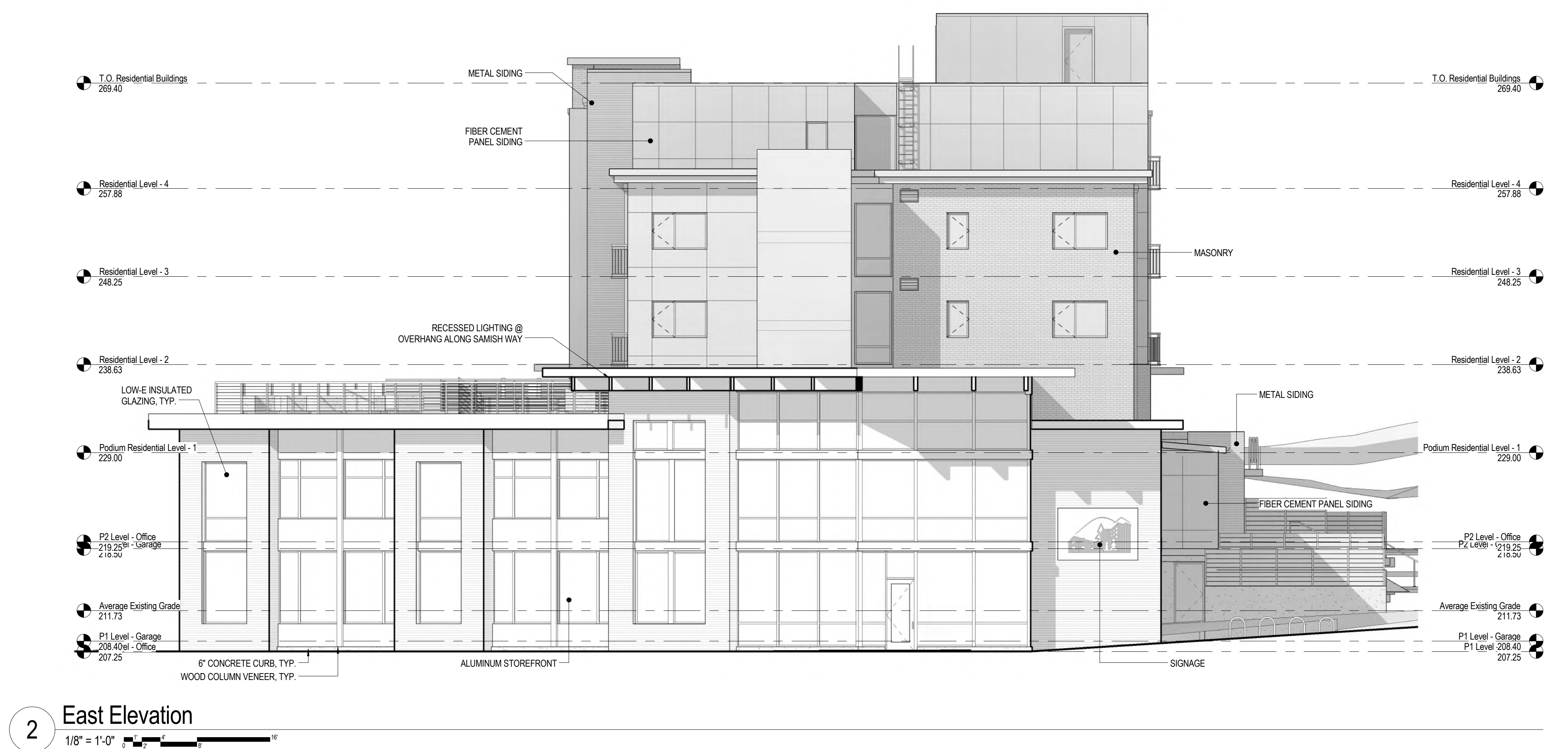
2 Roof Plan
1/8" = 1'-0"

Samish Way Redevelopment (Aloha!) PHASE-I
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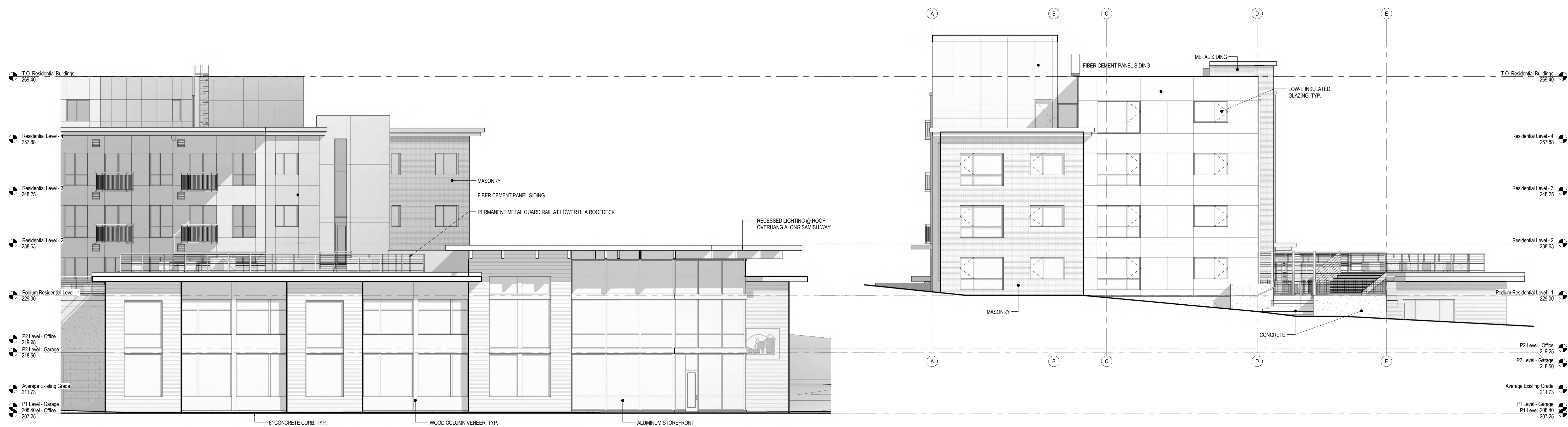
ROOF PLAN

A206

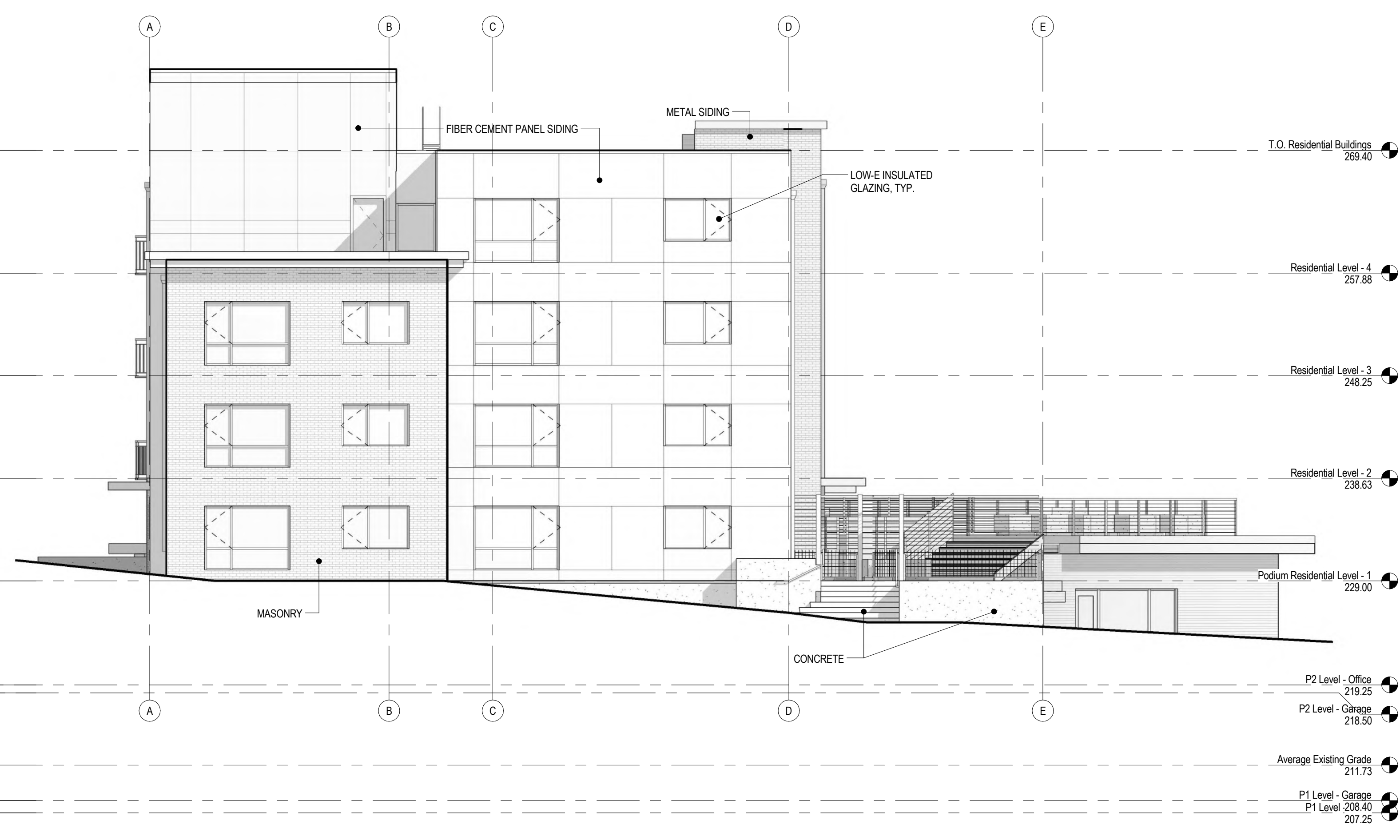




3 South Elevation
1/8" = 1'-0"



1 Southeast Elevation
1/8" = 1'-0"



2 West Elevation
1/8" = 1'-0"

Samish Way Redevelopment (Aloha!) PHASE-I

315 North Samish Way

Samish Way Redevelopment Partners LLLP

Job No.: 1571.01 Date: 17 JUL 2019
File No.: 1571.01 Phase I-BHA_DRB
Drawn By: RAW
Checked By: JMK
Issued for: DD MILESTONE #3

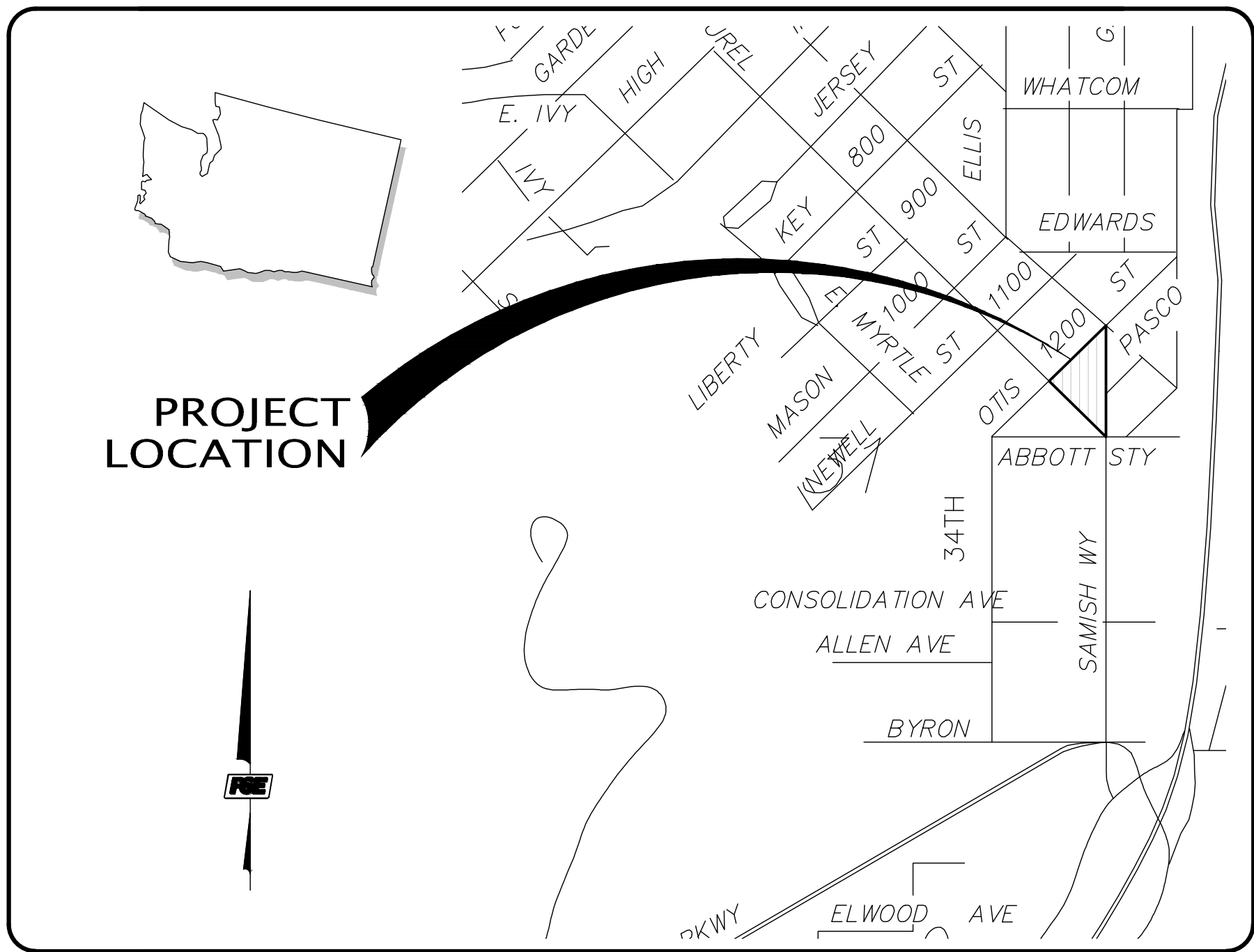
COLOR
PERSPECTIVE

A303



RMC Architects | Stephanie Bower, Architectural Illustration

SAMISH WAY REDEVELOPMENT



SHEET INDEX

- C101. COVER & NOTES
- C102. EXISTING CONDITONS
- C103. OTIS STREET GRADING & STORMWATER PLAN
- C104. OTIS STREET WATER & SANITARY SEWER PLAN
- C105. GRADING & STORMWATER PLAN
- C106. WATER & SANITARY SEWER PLAN
- C107. TEMPORARY EROSION AND SEDIMENT CONTROL PLAN
- C108. DETAILS
- C109. DETAILS
- C110. DETAILS
- C111. DETAILS
- C112. DETAILS
- C113. DETAILS
- C114. P1 LEVEL GRADING PLAN

LEGEND

EXISTING SYMBOL LEGEND

- = EXISTING PINE/SPRUCE TREE
- = EXISTING FIR TREE
- = EXISTING LANDSCAPE TREE/UNKNOWN SPECIES TREE
- = EXISTING ALDER TREE
- = EXISTING STUMP
- 15' 9" = DIAMETER OF EXISTING TREE
- = SPOT ELEVATION ON EXISTING GROUND
- = EXISTING AREA DRAIN
- = EXISTING SANITARY SEWER MANHOLE
- = EXISTING GATE VALVE
- = EXISTING WATER METER
- = EXISTING FIRE HYDRANT
- = EXISTING CONCRETE BOLLARD
- = EXISTING STOP SIGN
- = EXISTING STREET SIGN
- = EXISTING POWER POLE
- = EXISTING POWER POLE W/DROP
- = EXISTING YARD LIGHT
- = EXISTING GROUND GUY
- = EXISTING ASPHALT SURFACING TO REMAIN

PROPOSED SYMBOL LEGEND

- = PROPOSED STORM DRAIN MANHOLE
- = PROPOSED STORM DRAIN CLEANOUT
- = PROPOSED CATCH BASIN
- = PROPOSED ASPHALT SURFACING
- = PROPOSED LAWN SURFACING
- = PROPOSED FINISHED GRADE ELEVATION
- = PROPOSED GATE VALVE
- = PROPOSED WATER METER
- = PROPOSED FIRE HYDRANT
- = PROPOSED THRUST BLOCK
- = PROPOSED TAPPING TEE & VALVE
- 1 = CONSTRUCTION KEY NOTE
- = PROPOSED FINISHED GRADE ELEVATION
- = PROPOSED TOP OF CURB (TC) ELEVATION
- = PROPOSED FLOW LINE (FL) ELEVATION
- X = PROPOSED TREE REMOVAL

EXISTING LINE LEGEND

- = EXISTING RIGHT OF WAY LINE
- = EXISTING RIGHT OF WAY CENTER LINE
- = EXISTING EASEMENT
- = EXISTING EDGE OF ASPHALT
- = EXISTING EDGE OF CONCRETE
- = EXISTING CONCRETE PAD
- = EXISTING EDGE OF GRAVEL ROAD
- = EXISTING CURB
- = EXISTING SIDEWALK
- = EXISTING STRIPE (2 YELLOW)
- = EXISTING STRIPE (SKIP WHITE)
- = EXISTING SANITARY SEWER GRAVITY LINE
- = EXISTING WATER LINE
- = EXISTING OVERHEAD ELECTRIC LINES
- = EXISTING OVERHEAD ELECTRIC & COMMUNICATION LINES
- = EXISTING UNDERGROUND TRAFFIC SIGNAL LINE
- = EXISTING UNDERGROUND POWER
- = EXISTING UNDERGROUND COMMUNICATIONS LINE
- = EXISTING UNDERGROUND GAS LINE
- = EXISTING TOP OF SLOPE LINE
- = EXISTING TOE OF SLOPE LINE
- = EXISTING GRADE INDEX CONTOUR
- = EXISTING GRADE INTERVAL CONTOUR
- = EXISTING FLOW LINE
- = EXISTING EDGE OF LANDSCAPED AREA
- = EXISTING CHAINLINK FENCE
- = EXISTING LOG BARRIER

PROPOSED LINE LEGEND

- = PROPOSED EDGE OF ASPHALT
- = PROPOSED EDGE OF GRAVEL
- = PROPOSED CURB
- = PROPOSED SAWCUT LIMITS
- = PROPOSED TOP OF SLOPE LINE
- = PROPOSED SLOPE DIRECTION ARROW
- = PROPOSED TOE OF SLOPE LINE
- = PROPOSED FINISHED GRADE INDEX CONTOUR
- = PROPOSED FINISHED GRADE INTERVAL CONTOUR
- = PROPOSED STORM DRAIN LINE
- = PROPOSED SANITARY SEWER LINE
- = PROPOSED WATER LINE

ABBREVIATIONS

ACP	ASPHALT CONCRETE PAVING	F/C	FACE OF CURB	R/W	RIGHT OF WAY
ADA	AMERICAN DISABILITIES ACT	FF	FINISH FLOOR	SD	STORM DRAIN
A.F.#	AUDITOR'S FILE NUMBER	FG	FINISH GRADE	STD	STANDARD
APWA	AMERICAN PUBLIC WORKS ASSOCIATION	F&G	FRAME AND GRATE	SDCB	STORM DRAIN CATCH BASIN
APPROX.	APPROXIMATE	FL	FLOWLINE	SDCO	STORM DRAIN CLEAN-OUT
A.D.	ALGEBRAIC DIFFERENCE	FS	FINISH SURFACE	SDR	STANDARD DIAMETER RATIO
B/C	BACK OF CURB	GA	GAUGE	SDMH	STORM DRAIN MANHOLE
BW	BOTTOM OF WALL	GALV.	GALVANIZED	SVC	SERVICE
BMP	BEST MANAGEMENT PRACTICE	H.C.	HANDICAP	SPEC.	SPECIFICATION
BVCS	BEGINNING VERTICAL CURVE STATION	HMA	HOT MIX ASPHALT	SSCO	SANITARY SEWER CLEAN-OUT
BVCE	BEGINNING VERTICAL CURVE ELEVATION	IE	INVERT ELEVATION	SSMH	SANITARY SEWER MANHOLE
CAVFS	COMPOST AMENDED VEGETATED	INV.	INVERT	STD	STANDARD
CB	CATCH BASIN	K	RATE OF VERTICAL CURVATURE	STA	STATION
C&G	CURB AND GUTTER	LT	LEFT	S/W	SIDEWALK
CL	CENTERLINE	LF	LINEAL FEET	SF	SQUARE FOOT
CMF	CORRUGATED METAL PIP	MAX.	MAXIMUM	T/W	TOP OF WALL
CMU	CONCRETE MASONRY UNIT	MIN.	MINIMUM	TBOC	TOP BACK OF CURB
CONC.	CONCRETE	N.I.C.	NOT IN CONTRACT	TC	TOP OF CURB
COP	CORRUGATED POLYETHYLENE PIPE	NVPA	NATIVE VEGETATION PROTECTION AREA	TF	TOP OF FOOTING
CSTC	CRUSHED SURFACING TOP COURSE	OHWM	ORDINARY HIGH WATER MARK	TP	TOP OF PIPE
DIA.	DIAMETER	PROP.	PROPOSED	TBM	TEMPORARY BENCH MARK
D.I.	DUCTILE IRON	POLY.	POLYETHYLENE	TPV	TYPICAL
EG	EXISTING GRADE	PORF.	PERFORATED	VC	VERTICAL CURVE LENGTH
EL.	ELEVATION	PCC	PORTLAND CEMENT CONCRETE	USPS	UNITED STATES POSTAL SERVICE
EOP	EDGE OF PAVEMENT	PVC	POLYVINYL CHLORIDE	WM	WATER METER
EVCS	ENDING VERTICAL CURVE STATION	PVI	POINT OF VERTICAL INFLECTION	WSDOE	WASHINGTON STATE DEPT. OF ECOLOGY
EVCE	ENDING VERTICAL CURVE ELEVATION	PC	POINT OF CURVATURE	WSDOT	WASHINGTON STATE DEPT. OF TRANSPORTATION
EX.	EXISTING	PL	PROPERTY LINE	N	NORTH
F&G	FRAME AND GRATE	PT	POINT OF TANGENCY	E	EAST
F.HYD.	FIRE HYDRANT	R	CURVE RADIUS	S	SOUTH
		RT	RIGHT	W	WEST
		RET	RETAINING		

Samish Way Redevelopment (Aloha!) PHASE-I
315 North Samish Way
Samish Way Redevelopment Partners LLLP

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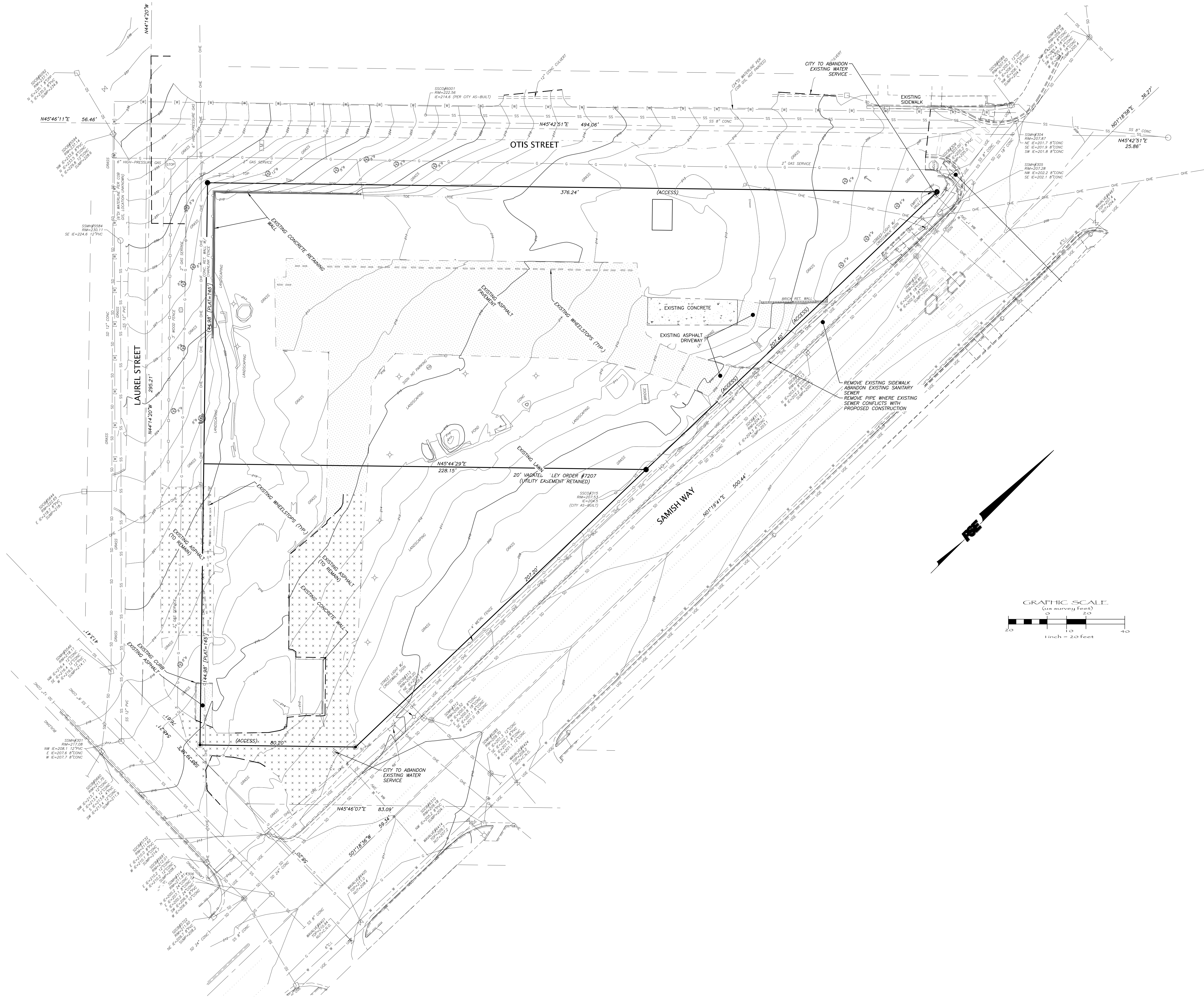
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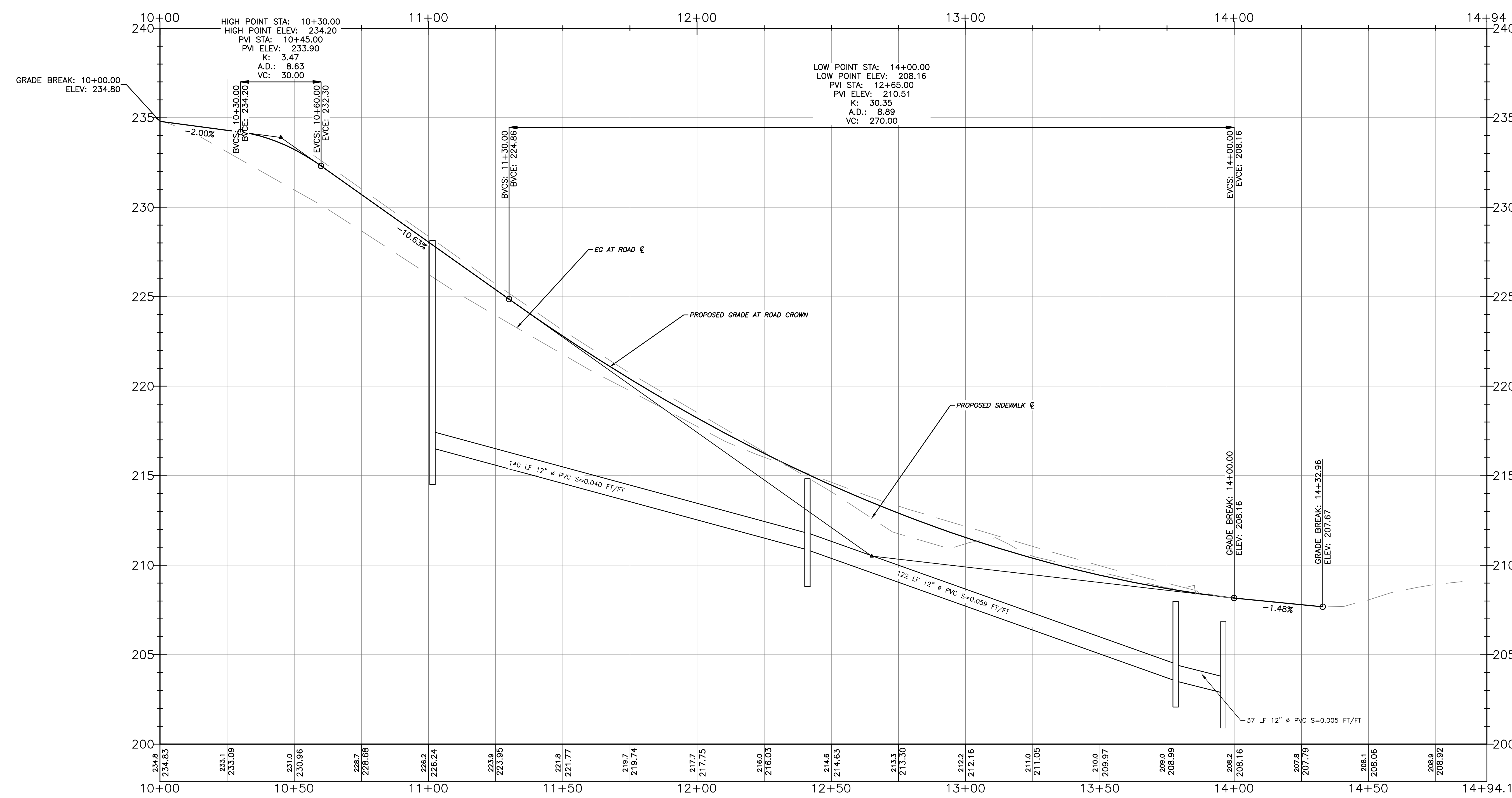
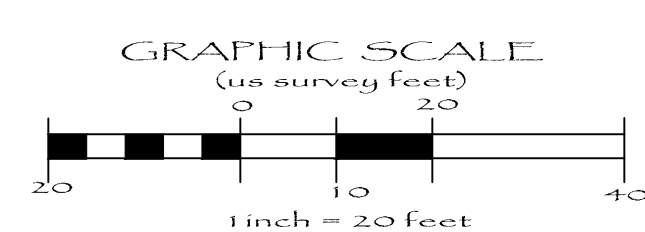
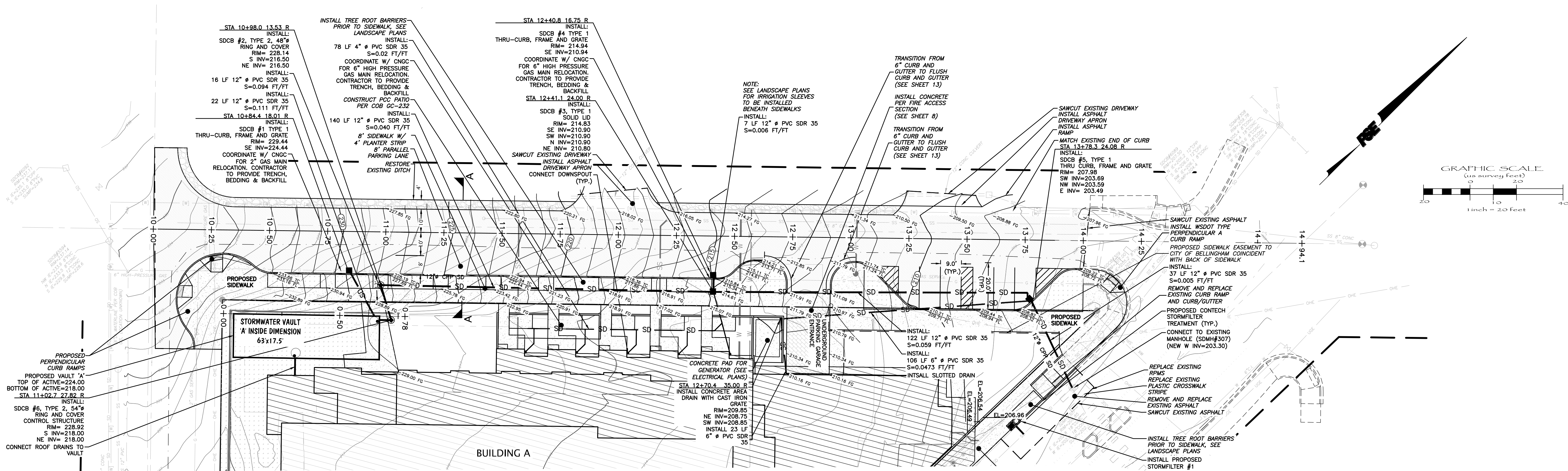
C101

RMC ARCHITECTS



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STORM VAULT PROFILE
1"=20' HORIZONTAL
1"=3' VERTICAL

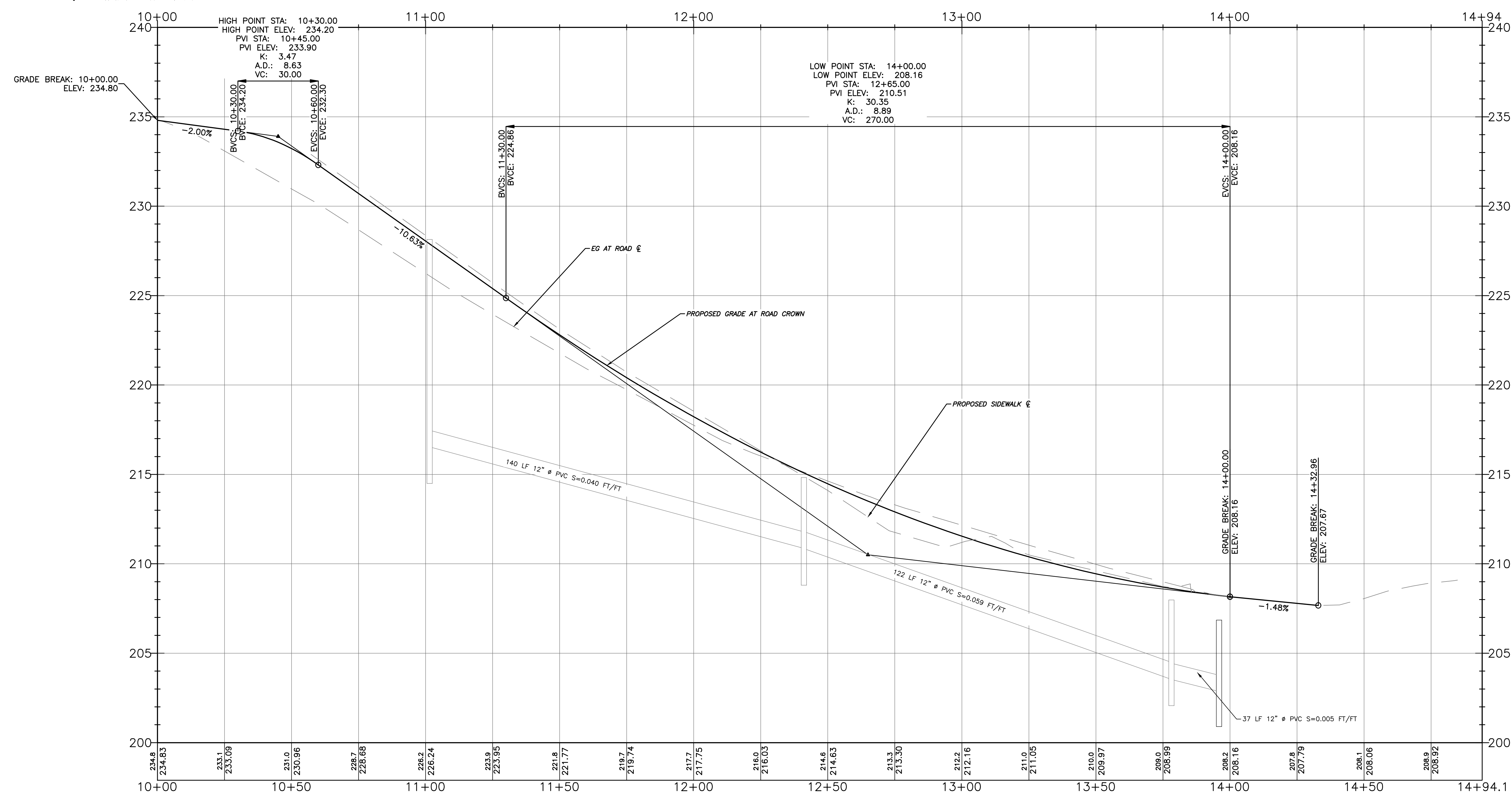
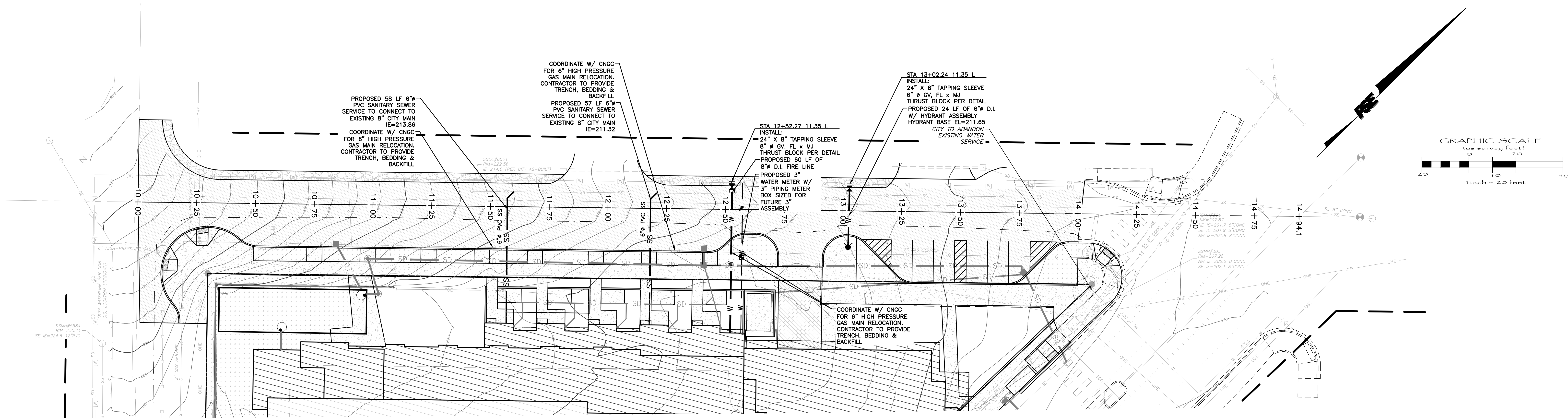
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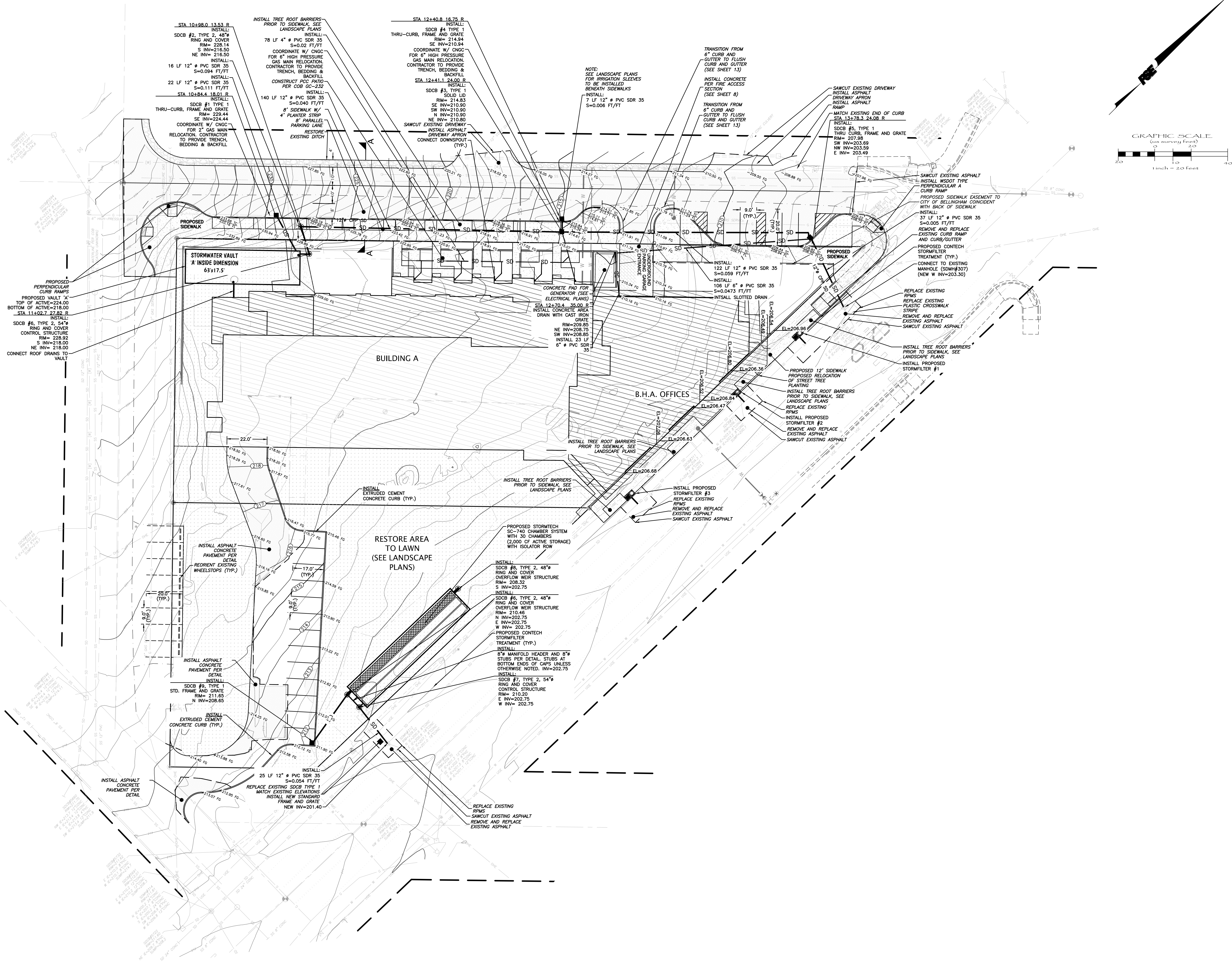
Samish Way Redevelopment (Aloha!) PHASE-I
315 North Samish Way
Samish Way Redevelopment Partners LLLP

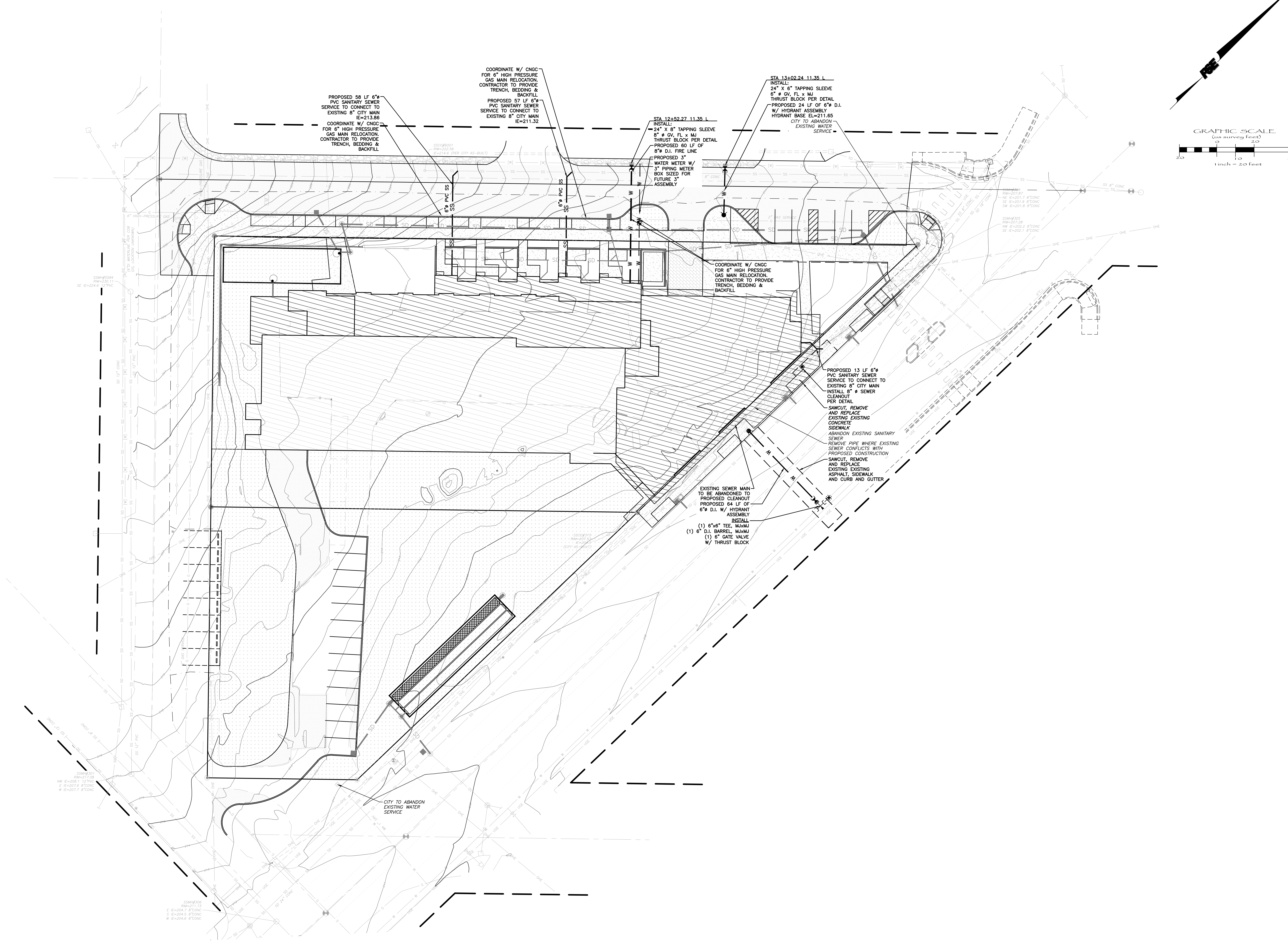
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File No.	2016112.scp	SP	DD
Drawn By	John Gimm		
Checked By	Jeff Vander Yacht		
Issued for	PERMIT DRAWINGS		

OTIS STREET
GRADING &
STORMWATER
PLAN
C103



STORM VAULT PROFILE
1"=20' HORIZONTAL
1"=3' VERTICAL





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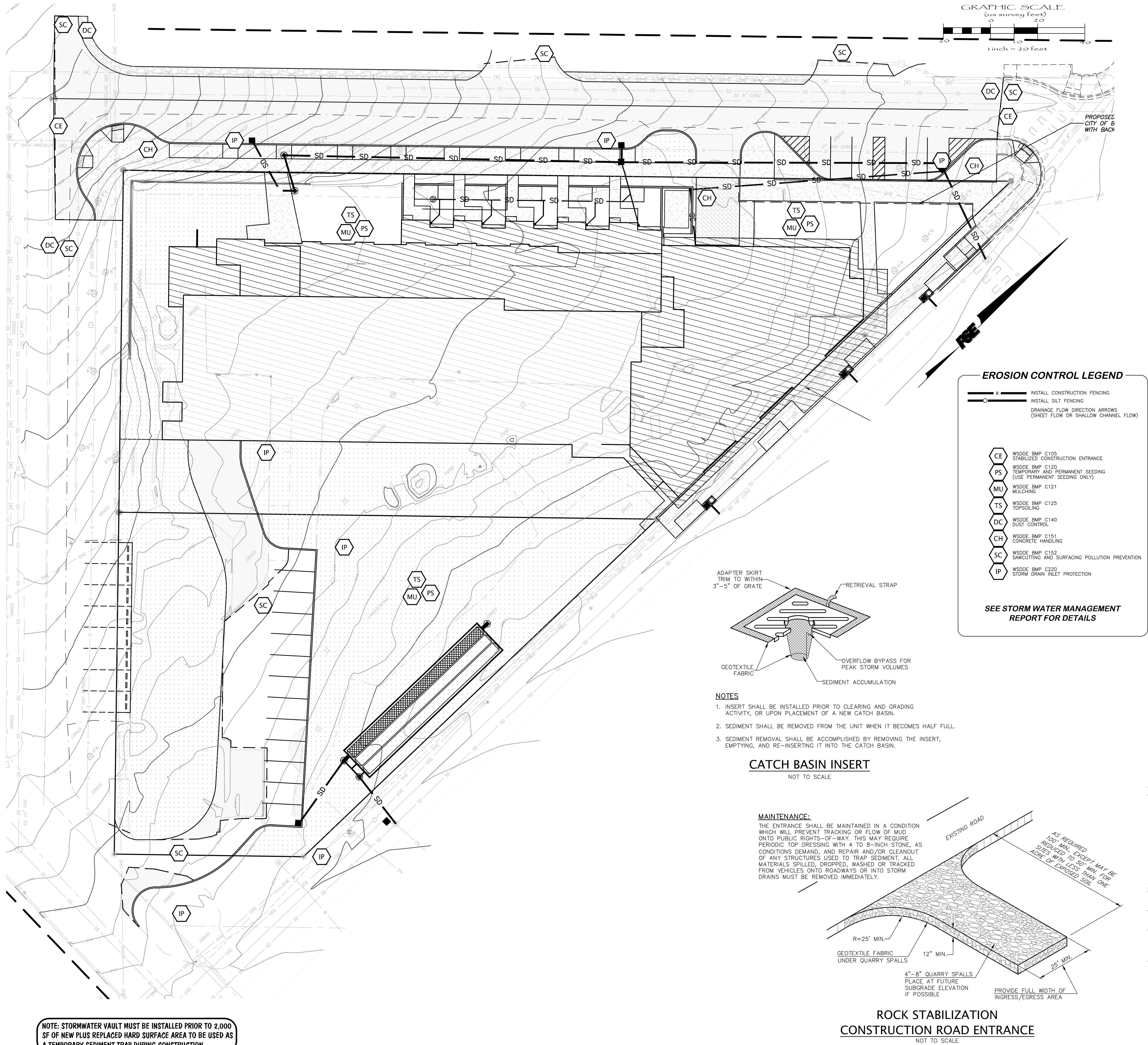


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315 North Samish Way
Samish Way Redevelopment Partners LLLP

Job No.	2016112	Date:	17 MAY 2019
File No.	2016112.scp	SP	DD
Drawn By:	Joel Gimm		
Checked By:	Jeff Vander Yacht		
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WATER &
SANITARY SEWER
PLAN

C106

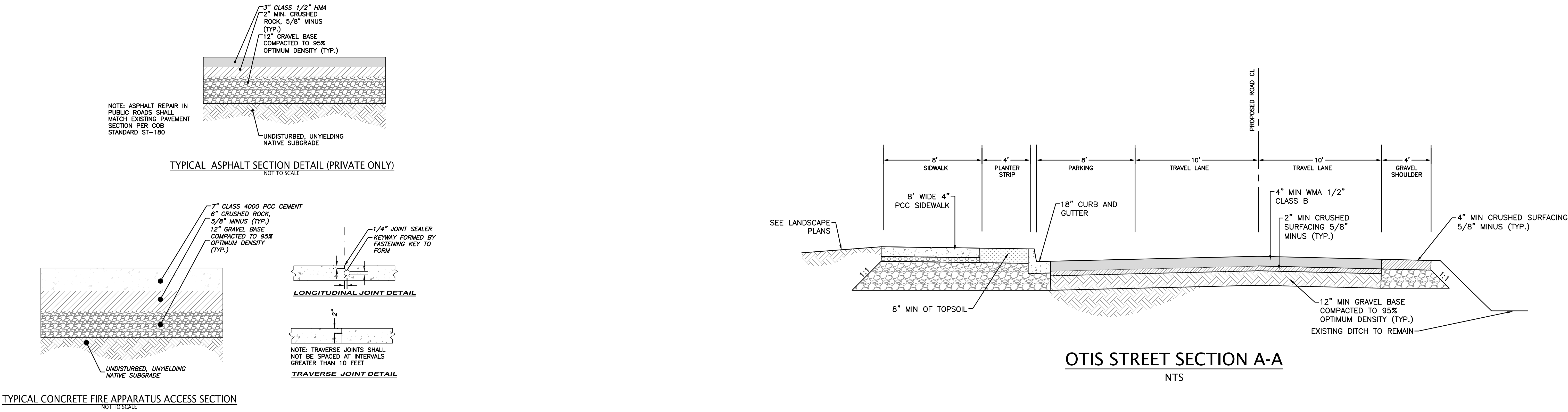


TEMPORARY EROSION/SEDIMENTATION CONTROL

1. A COPY OF THESE APPROVED PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
2. APPROVAL OF THESE TEMPORARY EROSION/SEDIMENTATION CONTROL (TESC) PLANS DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G., SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).
3. THE IMPLEMENTATION OF THESE TESC PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT AND UPGRADING OF THESE TESC FACILITIES IS THE RESPONSIBILITY OF THE CONTRACTOR UNTIL ALL CONSTRUCTION IS APPROVED.
4. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED IN THE FIELD PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE FLAGGED CLEARING LIMITS SHALL BE PERMITTED. THE FLAGGING SHALL BE MAINTAINED BY THE CONTRACTOR FOR THE DURATION OF CONSTRUCTION.
5. THE TESC FACILITIES SHOWN ON THE PLANS MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM OR VIOLATE APPLICABLE WATER STANDARDS.
6. THE TESC FACILITIES SHOWN ON THE PLANS ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE TESC FACILITIES SHALL BE UPGRADED (E.G., ADDITIONAL SUMPS, RELOCATION OF DITCHES AND SILT FENCES, ETC.) AS NEEDED FOR UNEXPECTED STORM EVENTS.
7. THE TESC FACILITIES SHALL BE INSPECTED DAILY BY THE CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING. A RECORD OF EACH INSPECTION AND ANY CORRECTIVE ACTION TAKEN MUST BE RETAINED WITH THE SWPPP.
8. ANY AREA STRIPPED OF VEGETATION, INCLUDING ROADWAY EMBANKMENTS, WHERE NO FURTHER WORK IS ANTICIPATED FOR A PERIOD OF 7 DAYS, SHALL BE IMMEDIATELY STABILIZED WITH THE APPROVED TESC METHODS (E.G., SEEDING, MULCHING, NETTING, EROSION BLANKETS, ETC.).
9. ANY AREA NEEDING TESC MEASURES, NOT REQUIRING IMMEDIATE ATTENTION, SHALL BE ADDRESSED WITHIN FIFTEEN (15) DAYS.
10. THE TESC FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN THE 24 HOURS FOLLOWING A STORM EVENT WITH THE APPROVAL OF LUMMI NATURAL RESOURCES DEPARTMENT.
11. AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSTREAM SYSTEM.
12. STABILIZED CONSTRUCTION ENTRANCES AND WASH PADS SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF CONSTRUCTION.
13. DURING THE TIME PERIOD OF OCTOBER 1 THROUGH APRIL 30, ALL PROJECT DISTURBED AREAS GREATER THAN 5,000 SQUARE FEET, THAT ARE TO BE LEFT UNWORKED FOR MORE THAN 12 HOURS, SHALL BE COVERED BY ONE OF THE FOLLOWING COVER MEASURES: MULCH, SODDING, OR PLASTIC.
14. ANY PERMANENT RETENTION/DETENTION FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE PERMANENT FACILITY IS TO FUNCTION ULTIMATELY AS AN INFILTRATION OR DISPERSION SYSTEM, THE FACILITY SHALL NOT BE USED AS A TEMPORARY SETTLING BASIN. NO UNDERGROUND DETENTION TANK, DETENTION VAULT, OR SYSTEM WHICH BACKS UNDER OR INTO A POND SHALL BE USED AS A TEMPORARY SETTLING BASIN.
15. WHERE SEEDING FOR TEMPORARY EROSION CONTROL IS REQUIRED, FAST GERMINATING GRASSES SHALL BE APPLIED AT AN APPROPRIATE RATE (E.G. ANNUAL OR PERENNIAL RYE APPLIED AT APPROXIMATELY 120 POUNDS PER ACRE).
16. WHERE STRAW MULCH FOR TEMPORARY EROSION CONTROL IS REQUIRED, IT SHALL BE APPLIED AT A MINIMUM THICKNESS OF TWO INCHES.
17. ALL EROSION/SEDIMENTATION CONTROL PONDS WITH A DEAD STORAGE DEPTH EXCEEDING 6 INCHES MUST HAVE SLOPES NOT STEEPER THAN 3H:1V.
18. ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH WASHINGTON STATE DEPARTMENT OF TRANSPORTATION STANDARDS AND SPECIFICATIONS.
19. EROSION/SEDIMENTATION CONTROL FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAILS IN THESE PLANS. LOCATIONS MAY BE MOVED TO SUIT FIELD CONDITIONS, SUBJECT TO APPROVAL BY THE ENGINEER AND BY THE CITY OF BELLINGHAM PUBLIC WORKS DEPARTMENT.

NPDES NOTES

1. THE CONTRACTOR SHALL KEEP A RECORD OF THE DATES WHEN MAJOR GRADING ACTIVITIES OCCUR, WHEN CONSTRUCTION ACTIVITIES TEMPORARILY OR PERMANENTLY CEASE ON A PORTION OF THE SITE, AND WHEN STABILIZATION MEASURES ARE IMPLEMENTED.
2. ALL EROSION CONTROL FACILITIES SHALL BE INSPECTED, MAINTAINED AND REPAIRED BY THE CONTRACTOR AS NEEDED TO ASSURE CONTINUED PERFORMANCE OF THEIR INTENDED FUNCTION. ALL ON SITE EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE INSPECTED AT LEAST ONCE EVERY SEVEN DAYS AND WITHIN 24 HOURS AFTER ANY STORM EVENT OF GREATER THAN 0.2 INCHES PER 24 HOUR PERIOD. AN INSPECTION REPORT FILE SHALL BE MAINTAINED BY THE CONTRACTOR FOR EACH INSPECTION.



STORMWATER POLLUTION PREVENTION PLAN - 13 ELEMENTS

ELEMENT #1 - MARK CLEARING LIMITS

PRIOR TO BEGINNING LAND DISTURBING ACTIVITIES, INCLUDING CLEARING AND GRADING, ALL CLEARING LIMITS, SENSITIVE AREAS AND THEIR BUFFERS, AND TREES THAT ARE TO BE PRESERVED WITHIN THE CONSTRUCTION AREA SHOULD BE CLEARLY MARKED, BOTH IN THE FIELD AND ON THE PLANS, TO PREVENT DAMAGE AND OFFSITE IMPACTS. PLASTIC, METAL, OR STAKE WIRE FENCE MAY BE USED TO MARK THE CLEARING LIMITS.

ELEMENT #2 - ESTABLISH CONSTRUCTION ACCESS

- CONSTRUCTION VEHICLE ACCESS AND EXIT SHALL BE DIRECTED TO THE PROPOSED DRIVEWAY LOCATIONS OFF OTIS STREET.
- ACCESS POINTS SHALL BE STABILIZED WITH QUARRY SPALL OR CRUSHED ROCK TO MINIMIZE THE TRACKING OF SEDIMENT ONTO PUBLIC ROADS PER BMP C105: STABILIZED CONSTRUCTION ENTRANCE.
- WHEEL WASH OR TIRE BATHS ARE NOT ANTICIPATED TO BE NEEDED FOR THIS PROJECT.
- PUBLIC ROADS SHALL AT A MINIMUM BE CLEANED THOROUGHLY AT THE END OF EACH DAY. SEDIMENT SHALL BE REMOVED FROM ROADS BY SHOVELING OR PICKUP SWEEPING AND SHALL BE TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA. STREET WASHING WILL BE ALLOWED ONLY AFTER SEDIMENT IS REMOVED IN THIS MANNER.
- STREET WASH WASTEWATER SHALL BE CONTROLLED BY PUMPING BACK ON-SITE, OR OTHERWISE BE PREVENTED FROM DISCHARGING INTO SYSTEMS TRIBUTARY TO STATE SURFACE WATERS.

ELEMENT #3 - CONTROL FLOW RATES

- PROPERTIES AND WATERWAYS DOWNSTREAM FROM DEVELOPMENT SITES SHALL BE PROTECTED FROM EROSION DUE TO INCREASES IN THE VOLUME, VELOCITY, AND PEAK FLOW RATE OF STORMWATER RUNOFF FROM THE PROJECT SITE. PROPERTIES SUBJECT TO MINIMUM REQUIREMENT #5 AND/OR #7 SHALL IMPLEMENT CONTROLS AS EARLY IN THE DEVELOPMENT AS IS PRACTICABLE TO MITIGATE FOR FLOW RATES.
- DOWNSTREAM ANALYSIS IS NECESSARY IF CHANGES IN FLOWS COULD IMPAIR OR ALTER CONVEYANCE SYSTEMS, STREAM BANKS, BED SEDIMENT OR AQUATIC HABITAT. SEE THE ECOLOGY MANUAL FOR OFFSITE ANALYSIS GUIDANCE.
- WHERE NECESSARY TO COMPLY WITH MINIMUM REQUIREMENT #7, STORMWATER RETENTION/DETENTION FACILITIES SHALL BE CONSTRUCTED AS ONE OF THE FIRST STEPS IN GRADING. DETENTION FACILITIES SHALL BE FUNCTIONAL PRIOR TO CONSTRUCTION OF SITE IMPROVEMENTS (E.G. IMPERVIOUS SURFACES).
- IF PERMANENT INFILTRATION PONDS ARE USED FOR FLOW CONTROL DURING CONSTRUCTION, THESE FACILITIES SHOULD BE PROTECTED FROM SILTATION DURING THE CONSTRUCTION PHASE.

ELEMENT #4 - INSTALL SEDIMENT CONTROLS

- THE DUFF LAYER, NATIVE TOPSOIL, AND NATURAL VEGETATION SHALL BE RETAINED IN AN UNDISTURBED STATE TO THE MAXIMUM EXTENT PRACTICABLE.
- PRIOR TO LEAVING A CONSTRUCTION SITE, OR PRIOR TO DISCHARGE TO AN INFILTRATION FACILITY, STORMWATER RUNOFF FROM DISTURBED AREAS SHALL PASS THROUGH A SEDIMENT POND, DIRT BAG, OR ANOTHER APPROPRIATE SEDIMENT REMOVAL BMP THAT IS SHOWN IN THE TEMPORARY EROSION AND SEDIMENTATION CONTROL PLAN. RUNOFF FROM FULLY STABILIZED AREAS MAY BE DISCHARGED WITHOUT A SEDIMENT REMOVAL BMP, BUT MUST MEET THE FLOW CONTROL PERFORMANCE STANDARD OF ELEMENT 3 ABOVE. FULL STABILIZATION MEANS CONCRETE OR ASPHALT PAVING, QUARRY SPALLS USED AS DITCH LINING, OR THE USE OF ROLLED EROSION PRODUCTS, A BONDED FIBER MATRIX PRODUCT, OR VEGETATIVE COVER IN A MANNER THAT WILL FULLY PREVENT SOIL EROSION. SEDIMENT PONDS, VEGETATED BUFFER STRIPS, SEDIMENT BARRIERS OR FILTERS, DIKES, AND OTHER BMPs INTENDED TO TRAP SEDIMENT ON-SITE SHALL BE CONSTRUCTED AS ONE OF THE FIRST STEPS IN GRADING. THESE BMPs SHALL BE FUNCTIONAL BEFORE OTHER LAND DISTURBING ACTIVITIES TAKE PLACE.
- EARTHEN STRUCTURES SUCH AS DAMS, DIKES, AND DIVERSIONS SHALL BE SEEDED AND MULCHED ACCORDING TO THE TIMING INDICATED IN ELEMENT 5 BELOW.

1.1.5 ELEMENT #5 - STABILIZE SOILS

- ALL EXPOSED AND UNWORKED SOILS SHALL BE STABILIZED BY APPLICATION OF EFFECTIVE BMPs THAT PROTECT THE SOIL FROM THE EROSION FORCES OF RAINDROP IMPACT AND FLOWING WATER, AND WIND EROSION.
- FROM OCTOBER 1 THROUGH APRIL 30 OF EACH YEAR, NO SOILS SHALL REMAIN EXPOSED AND UNWORKED FOR MORE THAN 2 DAYS. FROM MAY 1 TO SEPTEMBER 30 OF EACH YEAR, NO SOILS SHALL REMAIN EXPOSED AND UNWORKED FOR MORE THAN 7 DAYS. THIS CONDITION APPLIES TO ALL SOILS ON SITE, WHETHER AT FINAL GRADE OR NOT.
- APPLICABLE PRACTICES INCLUDE, BUT ARE NOT LIMITED TO, TEMPORARY AND PERMANENT SEEDING, SODDING, MULCHING, PLASTIC COVERING, SOIL APPLICATION OF POLYACRYLAMIDE (PAM), EARLY APPLICATION OF GRAVEL BASE ON AREAS TO BE PAVED, AND DUST CONTROL.
- SOIL STABILIZATION MEASURES SELECTED SHOULD BE APPROPRIATE FOR THE TIME OF YEAR, SITE CONDITIONS, ESTIMATED DURATION OF USE, AND POTENTIAL WATER QUALITY IMPACTS THAT STABILIZATION AGENTS MAY HAVE ON DOWNSTREAM WATERS OR GROUND WATER.
- SOIL STOCKPILES MUST BE STABILIZED AND PROTECTED WITH SEDIMENT TRAPPING MEASURES.
- WORK ON LINEAR CONSTRUCTION SITES AND ACTIVITIES, INCLUDING RIGHT-OF-WAY AND EASEMENT CLEARING, ROADWAY DEVELOPMENT, PIPELINES, AND TRENCHING FOR UTILITIES, SHALL NOT EXCEED THE CAPABILITY OF THE INDIVIDUAL CONTRACTOR FOR HIS PORTION OF THE PROJECT TO INSTALL THE BEDDING MATERIALS, ROADBEDS, STRUCTURES, PIPELINES, AND/OR UTILITIES, AND TO RE-STABILIZE THE DISTURBED SOILS, MEETING THE TIMING CONDITIONS LISTED ABOVE.
- IN ADDITION, AT THE DISCRETION OF THE PUBLIC WORKS DIRECTOR THOSE SITES UNABLE TO MAINTAIN THE QUALITY OF THEIR STORMWATER DISCHARGE MAY BE REQUIRED TO PROVIDE SOIL STABILIZATION TO ALL EXPOSED SOIL AREAS REGARDLESS OF THE WORKING STATUS OF THE AREA. UPON WRITTEN NOTIFICATION, THE PROPERTY OWNER SHALL PROVIDE FULL STABILIZATION OF ALL EXPOSED SOIL AREAS WITHIN 24 HOURS.

THE MINIMUM REQUIRED SOIL STABILIZING WSDOE BMPs ARE C120: TEMPORARY AND PERMANENT SEEDING, C121 MULCHING, C140 DUST CONTROL. THE OPTIONAL SOIL STABILIZING BMPs ARE: C122 NETS AND BLANKETS, C123 PLASTIC COVERING.

ELEMENT #6 - PROTECT SLOPES

- CUT AND FILL SLOPES SHALL BE DESIGNED AND CONSTRUCTED IN A MANNER THAT WILL MINIMIZE EROSION.
- CONSIDER SOIL TYPE AND ITS POTENTIAL FOR EROSION.
- REDUCE SLOPE RUNOFF VELOCITIES BY REDUCING THE CONTINUOUS LENGTH OF SLOPE WITH TERRACING AND DIVERSIONS, REDUCE SLOPE STEEPNESS, AND ROUGHEN SLOPE SURFACE.
- DIVERT UPSLOPE DRAINAGE AND RUN-ON WATERS FROM OFF-SITE WITH INTERCEPTORS AT TOP OF SLOPE. OFF-SITE STORMWATER SHOULD BE HANDLED SEPARATELY FROM STORMWATER GENERATED ON THE SITE. DIVERSION OF OFF-SITE STORMWATER AROUND THE SITE MAY BE A VIABLE OPTION. DIVERTED FLOWS SHALL BE REDIRECTED TO THE NATURAL DRAINAGE LOCATION AT OR BEFORE THE PROPERTY BOUNDARY.
- CONTAIN DOWN SLOPE COLLECTED FLOWS IN PIPES, SLOPE DRAINS, OR PROTECTED CHANNELS.
- PROVIDE DRAINAGE TO REMOVE GROUND WATER INTERSECTING THE SLOPE SURFACE OF EXPOSED SOIL AREAS.
- EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF TRENCHES, CONSISTENT WITH SAFETY AND SPACE CONSIDERATIONS.
- CHECK DAMS SHALL BE PLACED AT REGULAR INTERVALS WITHIN TRENCHES THAT ARE CUT DOWN A SLOPE.
- STABILIZE SOILS ON SLOPES, AS SPECIFIED IN ELEMENT #5.

IN ADDITION TO BMPs LISTED IN ELEMENT #5 ABOVE, IF REQUIRED THE MINIMUM SLOPE PROTECTION BMPs ARE: C120 TEMPORARY AND PERMANENT SEEDING, C121 MULCHING.

ELEMENT #7 - PROTECT DRAIN INLETS

- ALL STORM DRAIN INLETS MADE OPERABLE DURING CONSTRUCTION SHALL BE PROTECTED SO THAT STORMWATER RUNOFF SHALL NOT ENTER THE CONVEYANCE SYSTEM WITHOUT FIRST BEING FILTERED OR TREATED TO REMOVE SEDIMENT. THE REQUIRED BMP IS: C220 STORM DRAIN INLET PROTECTION
- ALL APPROACH ROADS SHALL BE KEPT CLEAN, AND ALL SEDIMENT AND STREET WASH WATER SHALL NOT BE ALLOWED TO ENTER STORM DRAINS WITHOUT PRIOR AND ADEQUATE TREATMENT UNLESS TREATMENT IS PROVIDED BEFORE THE STORM DRAIN DISCHARGES TO WATERS OF THE STATE.

ELEMENT #8 - STABILIZE CHANNELS AND OUTLETS

- IF TEMPORARY OPEN CHANNELS ARE CONSTRUCTED THEY SHALL BE DESIGNED, CONSTRUCTED AND STABILIZED TO PREVENT EROSION FROM THE EXPECTED VELOCITY OF FLOW FROM A 2 YEAR, 24-HOUR FREQUENCY STORM FOR THE DEVELOPED CONDITION.
- STABILIZATION, INCLUDING ARMORING MATERIAL, ADEQUATE TO PREVENT EROSION OF OUTLETS, ADJACENT STREAM BANKS, SLOPES AND DOWNSTREAM REACHES SHALL BE PROVIDED AT THE OUTLETS OF ALL CONVEYANCE SYSTEMS.

THE REQUIRED BMP IS: C235: STRAW WATTLES

ELEMENT #9 - CONTROL POLLUTANTS

- ALL POLLUTANTS, INCLUDING WASTE MATERIALS AND DEMOLITION DEBRIS, THAT OCCUR ON-SITE DURING CONSTRUCTION SHALL BE HANDLED AND DISPOSED OF IN A MANNER THAT DOES NOT CAUSE CONTAMINATION OF STORMWATER.
- COVER, CONTAINMENT, AND PROTECTION FROM VANDALISM SHALL BE PROVIDED FOR ALL CHEMICALS, LIQUID PRODUCTS, PETROLEUM PRODUCTS, AND NON-INERT WASTES PRESENT ON THE SITE (SEE CHAPTER 173-304 WAC, AS CURRENTLY ENACTED OR HEREAFTER MODIFIED, FOR THE DEFINITION OF INERT WASTE, WHICH IS INCORPORATED HEREIN BY THIS REFERENCE).
- MAINTENANCE AND REPAIR OF HEAVY EQUIPMENT AND VEHICLES INVOLVING OIL CHANGES, HYDRAULIC SYSTEM DRAIN DOWN, SOLVENT AND DE-GREASING CLEANING OPERATIONS, FUEL TANK DRAIN DOWN AND REMOVAL, AND OTHER ACTIVITIES WHICH MAY RESULT IN DISCHARGE OR SPILLAGE OF POLLUTANTS TO THE GROUND OR INTO STORMWATER RUNOFF MUST BE CONDUCTED USING SPILL PREVENTION MEASURES. SUCH AS DRIP PANS. CONTAMINATED SURFACES SHALL BE CLEANED IMMEDIATELY FOLLOWING ANY DISCHARGE OR SPILL INCIDENT. EMERGENCY REPAIRS MAY BE PERFORMED ON-SITE USING TEMPORARY PLASTIC PLACED BENEATH AND, IF RAINING, OVER THE VEHICLE.
- THERE IS NO ANTICIPATED NEED FOR WHEEL WASH, OR TIRE BATH WASTEWATER, FOR THIS PROJECT. IF THE NEED WERE TO ARISE THE WHEEL WASH, OR TIRE BATH WASTEWATER SHALL BE DISCHARGED TO A SEPARATE ON-SITE TREATMENT SYSTEM OR TO THE SANITARY SEWER.
- APPLICATION OF AGRICULTURAL CHEMICALS, INCLUDING FERTILIZERS AND PESTICIDES, SHALL BE CONDUCTED IN A MANNER AND AT APPLICATION RATES THAT WILL NOT RESULT IN LOSS OF CHEMICAL TO STORMWATER RUNOFF. MANUFACTURERS' RECOMMENDATIONS SHALL BE FOLLOWED FOR APPLICATION RATES AND PROCEDURES. THERE IS NO ANTICIPATED USE FOR AGRICULTURAL CHEMICALS, INCLUDING FERTILIZERS AND PESTICIDES FOR THIS PROJECT.
- MANAGEMENT OF PH-MODIFYING SOURCES SHALL PREVENT CONTAMINATION OF RUNOFF AND STORMWATER COLLECTED ON THE SITE. THESE SOURCES INCLUDE, BUT ARE NOT LIMITED TO, BULK CEMENT, CEMENT KILN DUST, FLY ASH, NEW CONCRETE WASHING AND CURING WATERS, WASTE STREAMS GENERATED FROM CONCRETE GRINDING AND SAWING, EXPOSED AGGREGATE PROCESSES, AND CONCRETE PUMPING AND MIXER WASHOUT WATERS. THE MINIMUM REQUIRED BMPs ARE: C151 CONCRETE HANDLING, C152 SAWCUTTING AND SURFACING POLLUTION PREVENTION.

ELEMENT #10 - CONTROL DE-WATERING

- ALL FOUNDATION, VAULT, AND TRENCH DE-WATERING WATER, WHICH HAS SIMILAR CHARACTERISTICS TO STORMWATER RUNOFF AT THE SITE, SHALL BE DISCHARGED INTO A CONTROLLED CONVEYANCE SYSTEM, PRIOR TO DISCHARGE TO A SEDIMENT TRAP OR SEDIMENT POND. CHANNELS MUST BE STABILIZED, AS SPECIFIED IN ELEMENT #8.
- CLEAN, NON-TURBID DE-WATERING WATER, SUCH AS WELL-POINT GROUND WATER, CAN BE DISCHARGED TO SYSTEMS TRIBUTARY TO STATE SURFACE WATERS, AS SPECIFIED IN ELEMENT #8, PROVIDED THE DE-WATERING FLOW DOES NOT CAUSE EROSION OR FLOODING OF THE RECEIVING WATERS. THESE CLEAN WATERS SHOULD NOT BE ROUTED THROUGH SEDIMENT PONDS WITH STORMWATER.
- HIGHLY TURBID OR OTHERWISE CONTAMINATED DEWATERING WATER, SUCH AS FROM CONSTRUCTION EQUIPMENT OPERATION, CLAMSHELL DIGGING, CONCRETE TREMIE POUR, OR WORK INSIDE A COFFERDAM, SHALL BE HANDLED SEPARATELY FROM STORMWATER AT THE SITE.
- OTHER DISPOSAL OPTIONS, DEPENDING ON SITE CONSTRAINTS, MAY INCLUDE, BY WAY OF EXAMPLE: 1) INFILTRATION, 2) TRANSPORT OFF-SITE IN VEHICLE, SUCH AS A VACUUM FLUSH TRUCK, FOR LEGAL DISPOSAL IN A MANNER THAT DOES NOT POLLUTE STATE WATERS, 3) ON-SITE TREATMENT USING CHEMICAL TREATMENT OR OTHER SUITABLE TREATMENT TECHNOLOGIES.

ELEMENT #11 - MAINTAIN BMPs

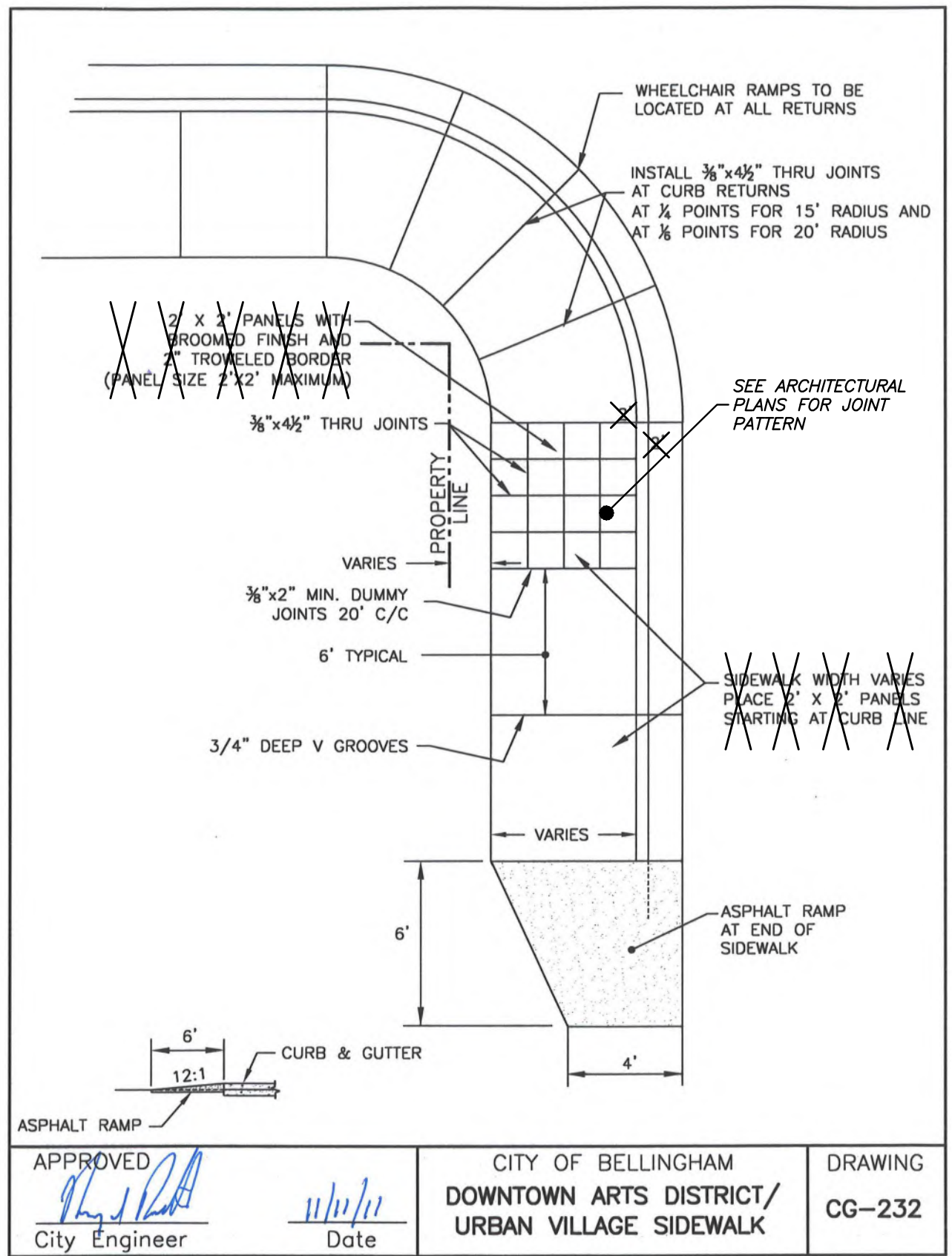
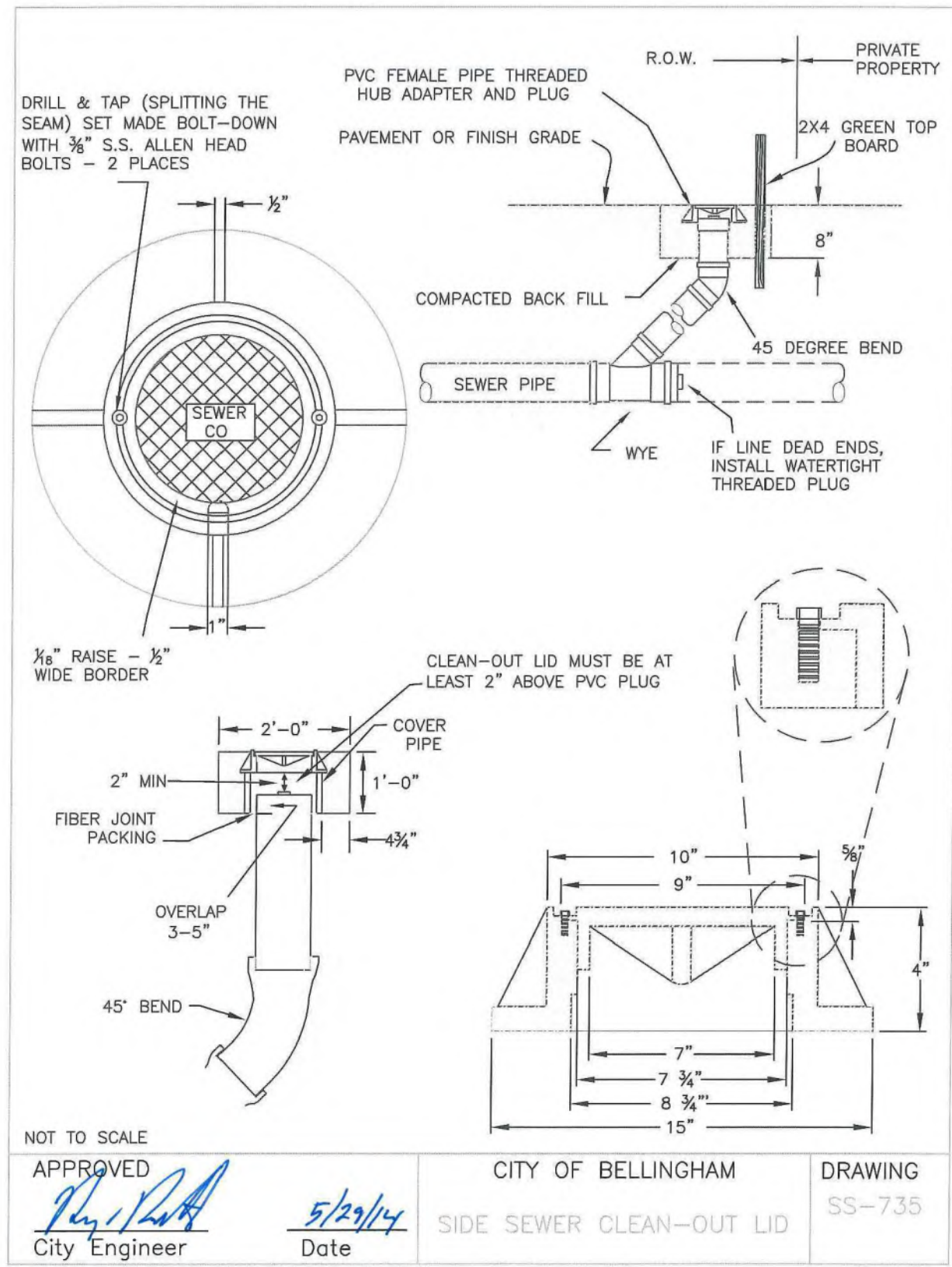
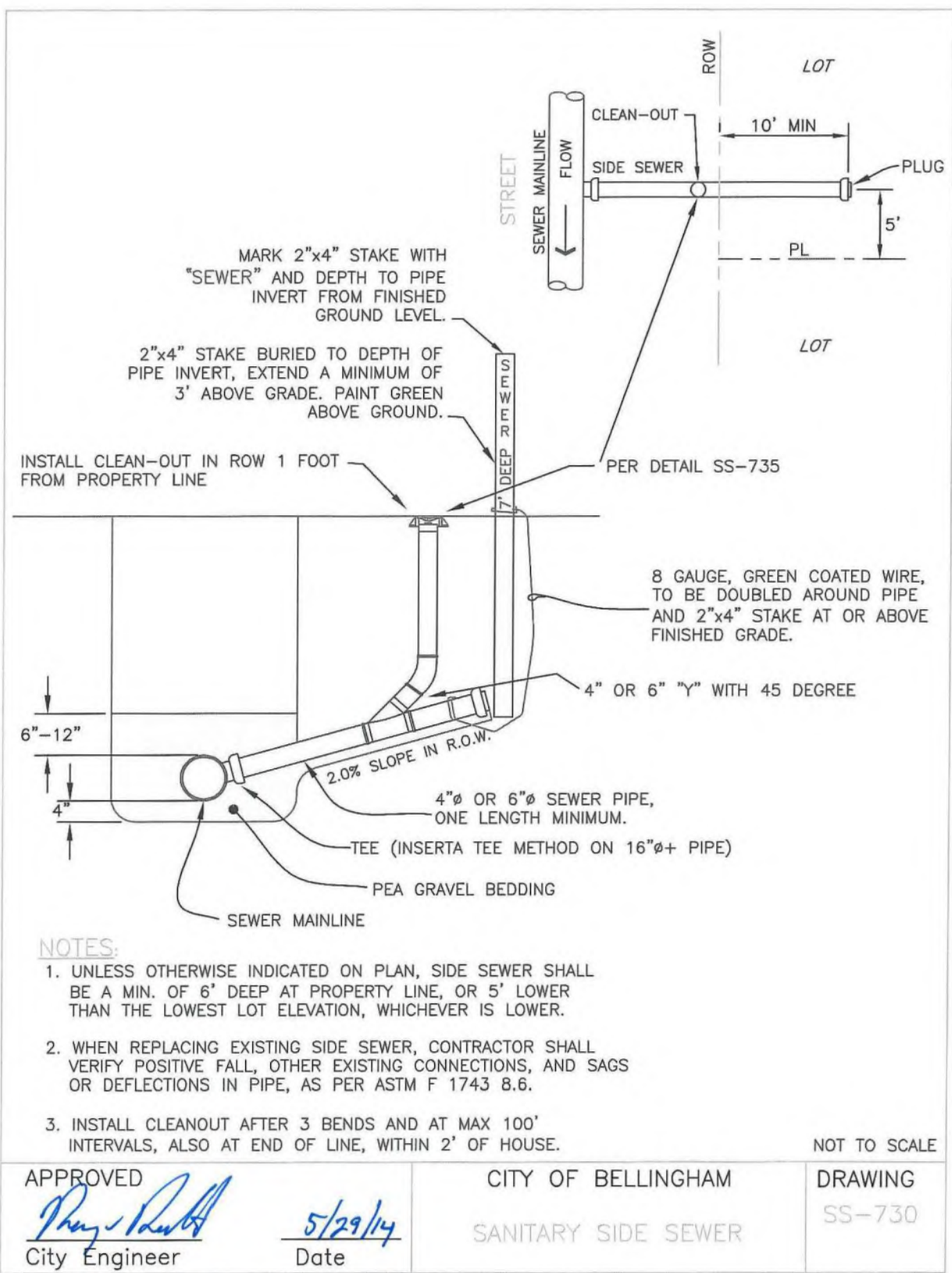
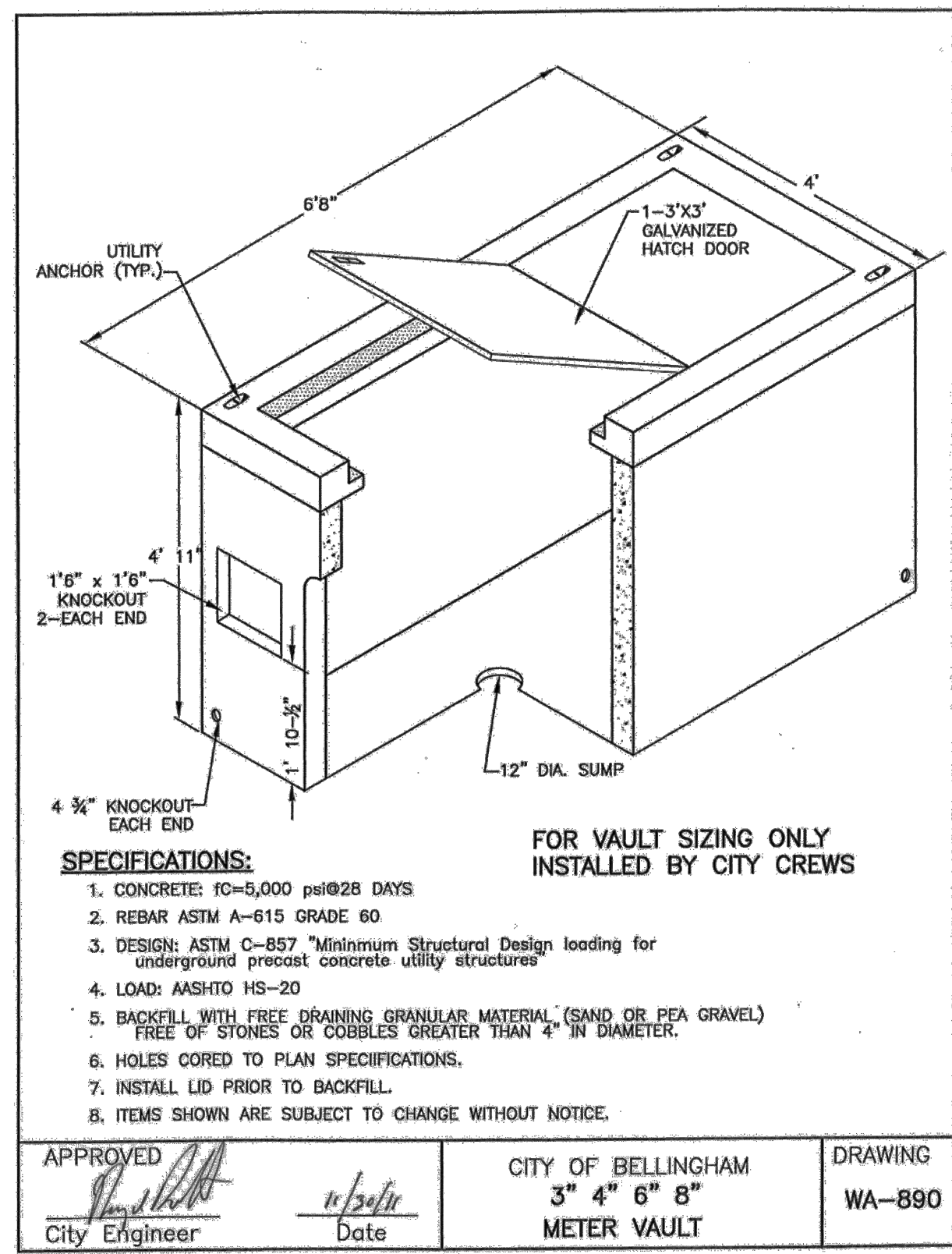
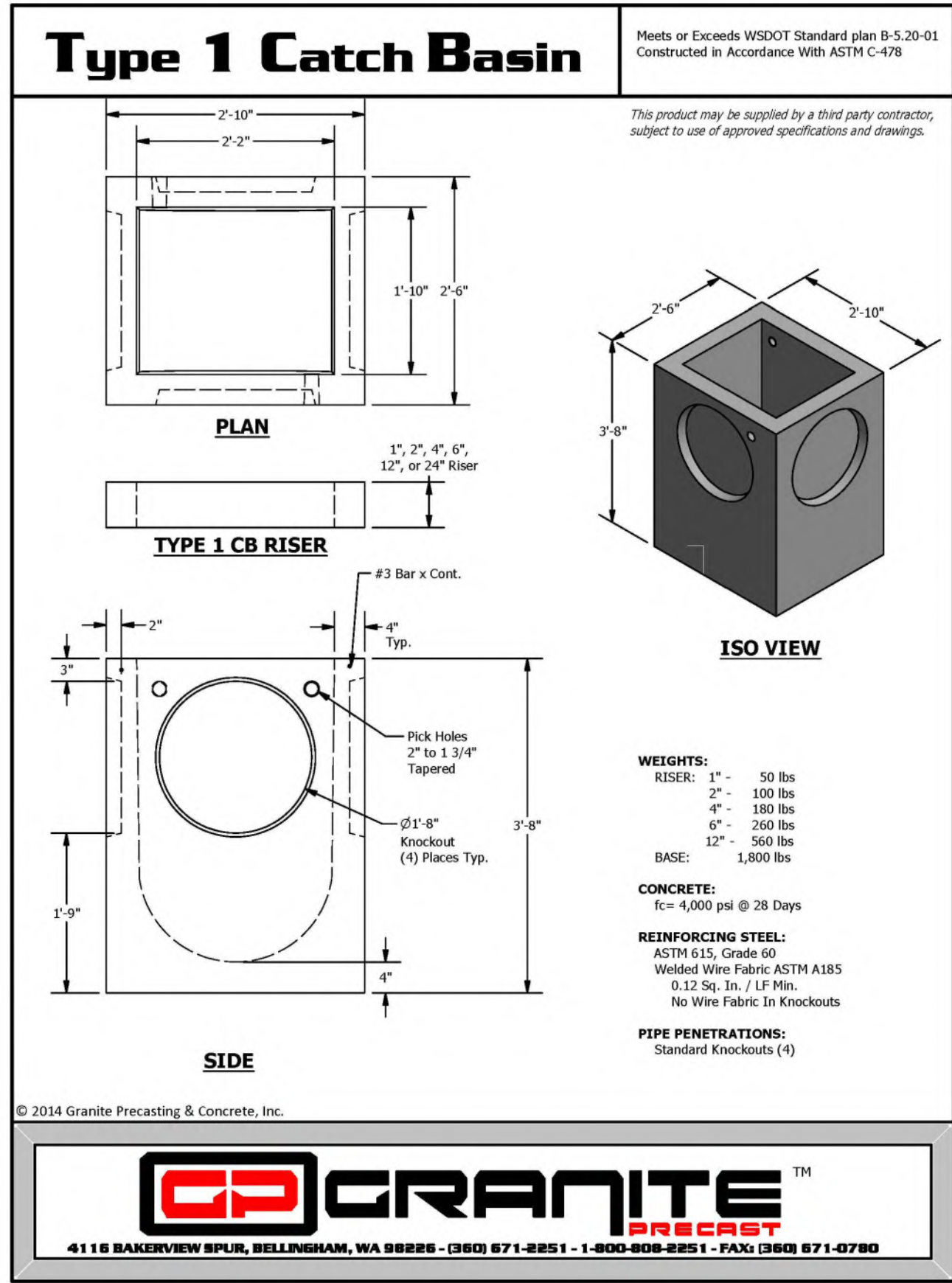
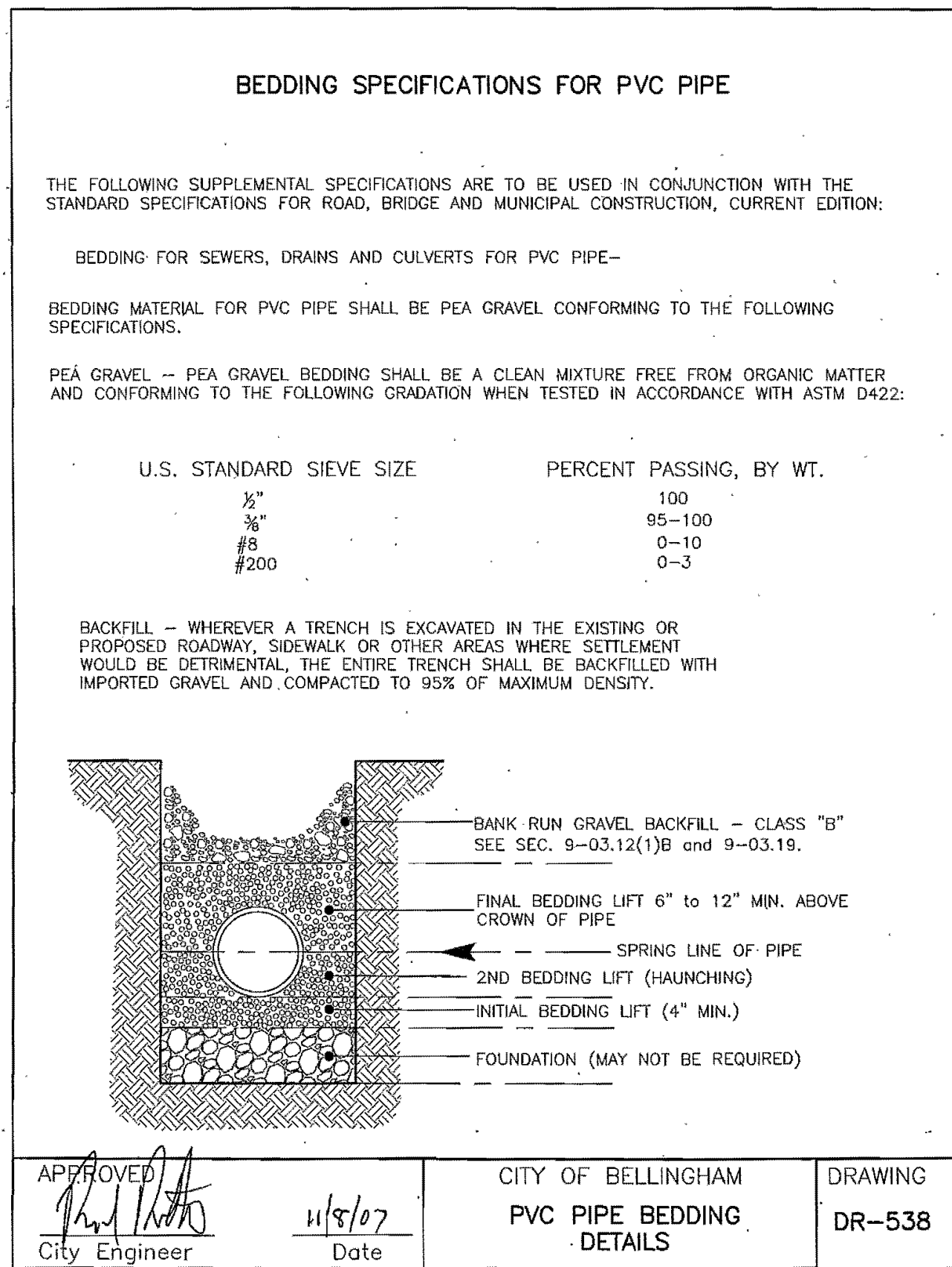
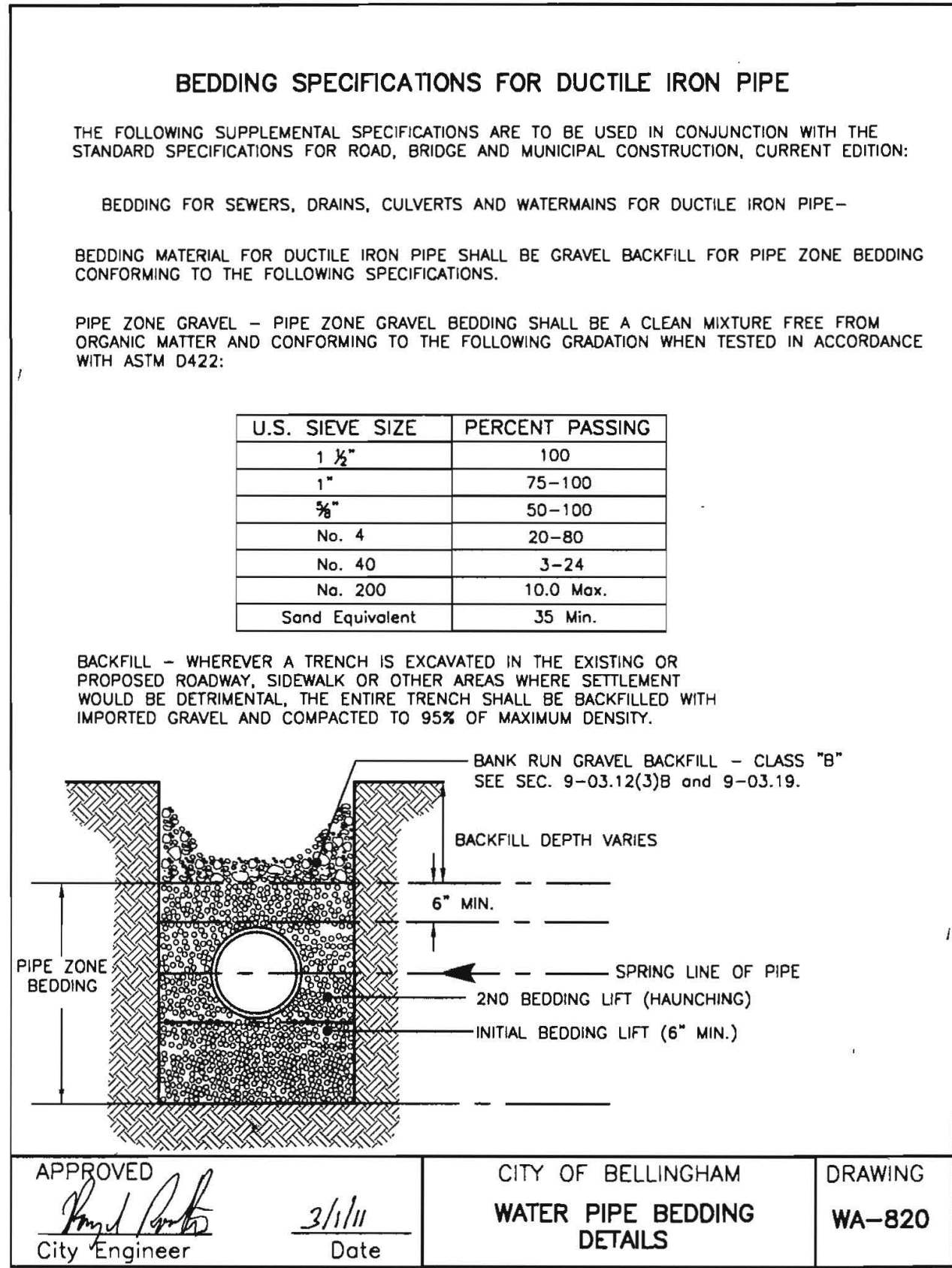
- ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL BMPs SHALL BE MAINTAINED AND REPAIRED AS NEEDED TO ASSURE CONTINUED PERFORMANCE OF THEIR INTENDED FUNCTION. ALL MAINTENANCE AND REPAIR SHALL BE CONDUCTED IN ACCORDANCE WITH BMPs.
- SEDIMENT CONTROL BMPs SHALL BE INSPECTED WEEKLY OR AFTER A RUNOFF-PRODUCING STORM EVENT DURING THE DRY SEASON AND DAILY DURING THE WET SEASON. ALL PROJECTS THAT DISTURB AN AREA GREATER THAN ONE ACRE SHALL HAVE A CERTIFIED EROSION CONTROL LEAD AVAILABLE TO THE SITE. THIS EROSION CONTROL LEAD SHALL BE RESPONSIBLE TO PROVIDE OVERVIEW OF ONGOING DAY TO DAY EROSION CONTROL REQUIREMENTS. THE EROSION CONTROL LEAD SHALL (WITHIN 24 HOURS) REPORT TO THE CITY AND DEPARTMENT OF ECOLOGY ANY SITE DISCHARGES THAT EXCEED STATE WATER QUALITY STANDARDS THAT HAVE OR ARE LIKELY TO HAVE ENTERED WATERS OF THE STATE.
- ALL TEMPORARY EROSION AND SEDIMENT CONTROL BMPs SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY BMPs ARE NO LONGER NEEDED. TRAPPED SEDIMENT SHALL BE REMOVED OR STABILIZED ON SITE. DISTURBED SOIL AREAS RESULTING FROM REMOVAL OF BMPs OR VEGETATION SHALL BE PERMANENTLY STABILIZED.

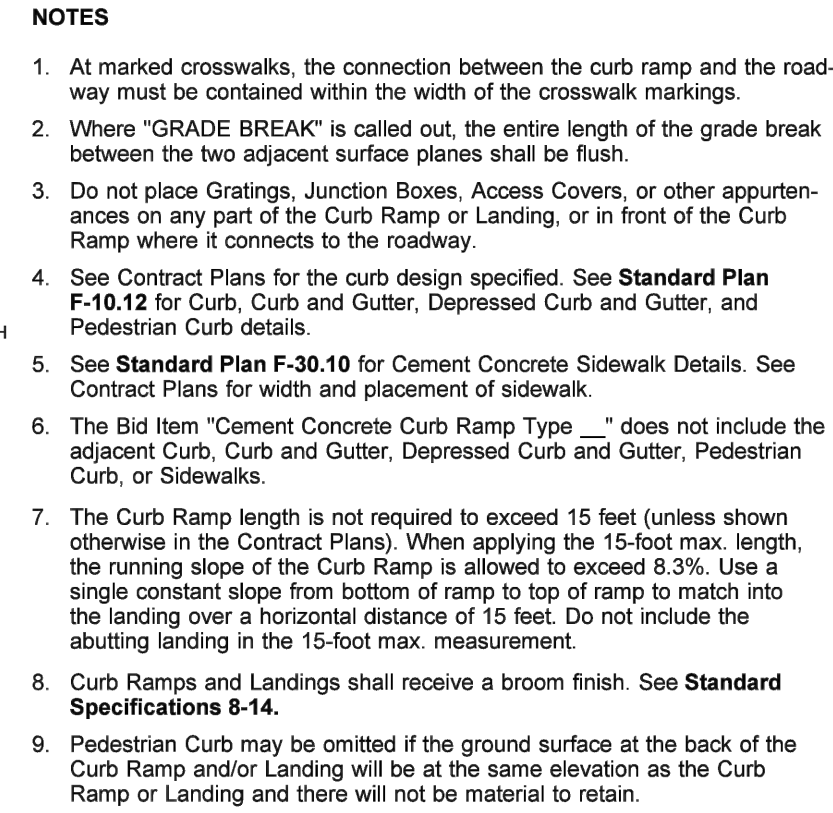
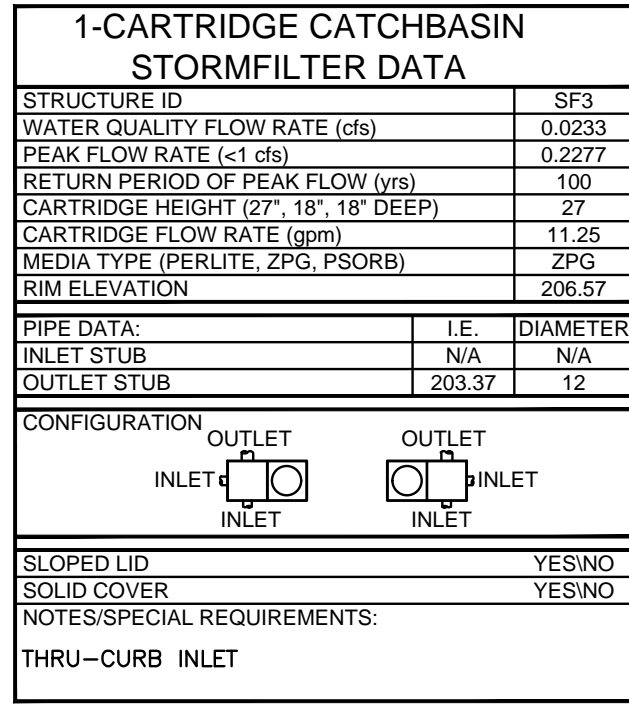
ELEMENT #12 - MANAGE THE PROJECT

- PHASING OF CONSTRUCTION - DEVELOPMENT PROJECTS SHALL BE PHASED WHERE FEASIBLE IN ORDER TO PREVENT, TO THE MAXIMUM EXTENT PRACTICABLE, THE TRANSPORT OF SEDIMENT FROM THE DEVELOPMENT SITE DURING CONSTRUCTION. REVEGETATION OF EXPOSED AREAS AND MAINTENANCE OF THAT VEGETATION SHALL BE AN INTEGRAL PART OF THE CLEARING ACTIVITIES FOR ANY PHASE.
- WHEN ESTABLISHING THESE PERMITTED CLEARING AND GRADING AREAS, CONSIDERATION SHOULD BE GIVEN TO MINIMIZING REMOVAL OF EXISTING TREES AND MINIMIZING DISTURBANCE/COMPACTION OF NATIVE SOILS EXCEPT AS NEEDED FOR BUILDING PURPOSES. PERMITTED CLEARING AND GRADING AREAS AND ANY OTHER AREAS REQUIRED TO PRESERVE CRITICAL OR SENSITIVE AREAS, BUFFERS, NATIVE GROWTH PROTECTION EASEMENTS, OR TREE RETENTION AREAS, SHALL BE DELINEATED ON THE SITE PLANS AND THE DEVELOPMENT SITE.
- COORDINATION WITH UTILITIES AND OTHER CONTRACTORS - THE PRIMARY PROJECT PROPONENT SHALL EVALUATE, WITH INPUT FROM UTILITIES AND OTHER CONTRACTORS, THE STORMWATER MANAGEMENT REQUIREMENTS FOR THE ENTIRE PROJECT, INCLUDING THE UTILITIES, WHEN PREPARING THE CONSTRUCTION SWPPP.
- INSPECTION AND MONITORING - ALL BMPs SHALL BE INSPECTED, MAINTAINED, AND REPAIRED AS NEEDED TO ASSURE CONTINUED PERFORMANCE OF THEIR INTENDED FUNCTION.
- FOR ANY PROJECT DISTURBING MORE THAN ONE ACRE, A CERTIFIED PROFESSIONAL IN EROSION AND SEDIMENT CONTROL SHALL BE IDENTIFIED IN THE CONSTRUCTION SWPPP AND SHALL BE ON-SITE OR ON-CALL AT ALL TIMES. CERTIFICATION MAY BE THROUGH THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION/ASSOCIATED GENERAL CONTRACTORS (WSDOT/AGC) CONSTRUCTION SITE EROSION AND SEDIMENT CONTROL CERTIFICATION PROGRAM OR ANY EQUIVALENT LOCAL OR NATIONAL CERTIFICATION AND/OR TRAINING PROGRAM, IN THE CITY'S DISCRETION.
- WHENEVER INSPECTION AND/OR MONITORING REVEALS THAT THE BMPs IDENTIFIED IN THE CONSTRUCTION SWPPP ARE INADEQUATE, DUE TO THE ACTUAL DISCHARGE OF OR POTENTIAL TO DISCHARGE A SIGNIFICANT AMOUNT OF ANY POLLUTANT, THE SWPPP SHALL BE MODIFIED, AS APPROPRIATE, IN A TIMELY MANNER.
- MAINTENANCE OF THE CONSTRUCTION SWPPP - THE CONSTRUCTION SWPPP SHALL BE RETAINED ON-SITE. THE CONSTRUCTION SWPPP SHALL BE MODIFIED WHENEVER THERE IS A SIGNIFICANT CHANGE IN THE DESIGN, CONSTRUCTION, OPERATION, OR MAINTENANCE OF ANY BMP.

ELEMENT #13 - PROTECT LOW IMPACT DEVELOPMENT BMPs

- PROTECT ALL BIORETENTION AND RAIN GARDEN BMPs FROM SEDIMENTATION THROUGH INSTALLATION AND MAINTENANCE OF EROSION AND SEDIMENT CONTROL BMPs ON PORTIONS OF THE SITE THAT DRAIN INTO THE BIORETENTION AND/OR RAIN GARDEN BMPs. RESTORE THE BMPs TO THEIR FULLY FUNCTIONING CONDITION IF THEY ACCUMULATE SEDIMENT DURING CONSTRUCTION. RESTORING THE BMP MUST INCLUDE REMOVAL OF SEDIMENT AND ANY SEDIMENT-LADEN BIORETENTION/RAIN GARDEN SOILS, AND REPLACING THE REMOVED SOILS WITH SOILS MEETING THE DESIGN SPECIFICATION.
- PREVENT COMPACTING BIORETENTION AND RAIN GARDEN BMPs BY EXCLUDING CONSTRUCTION EQUIPMENT AND FOOT TRAFFIC. PROTECT COMPLETED LAWN AND LANDSCAPED AREAS FROM COMPACTION DUE TO CONSTRUCTION EQUIPMENT.
- CONTROL EROSION AND AVOID INTRODUCING SEDIMENT FROM SURROUNDING LAND USES ONTO PERMEABLE PAVEMENTS. DO NOT ALLOW MUDDY CONSTRUCTION EQUIPMENT ON THE BASE MATERIAL OR PAVEMENT. DO NOT ALLOW SEDIMENT-LADEN RUNOFF ONTO PERMEABLE PAVEMENTS OR BASE MATERIALS.
- PAVEMENT FOULED WITH SEDIMENTS OR NO LONGER PASSING AN INITIAL INFILTRATION TEST MUST BE CLEANED USING PROCEDURES IN ACCORDANCE WITH THE ECOLOGY MANUAL OR THE MANUFACTURER'S PROCEDURES.
- KEEP ALL HEAVY EQUIPMENT OFF EXISTING SOILS UNDER LID FACILITIES THAT HAVE BEEN EXCAVATED TO FINAL GRADE TO RETAIN THE INFILTRATION RATE OF THE SOILS.






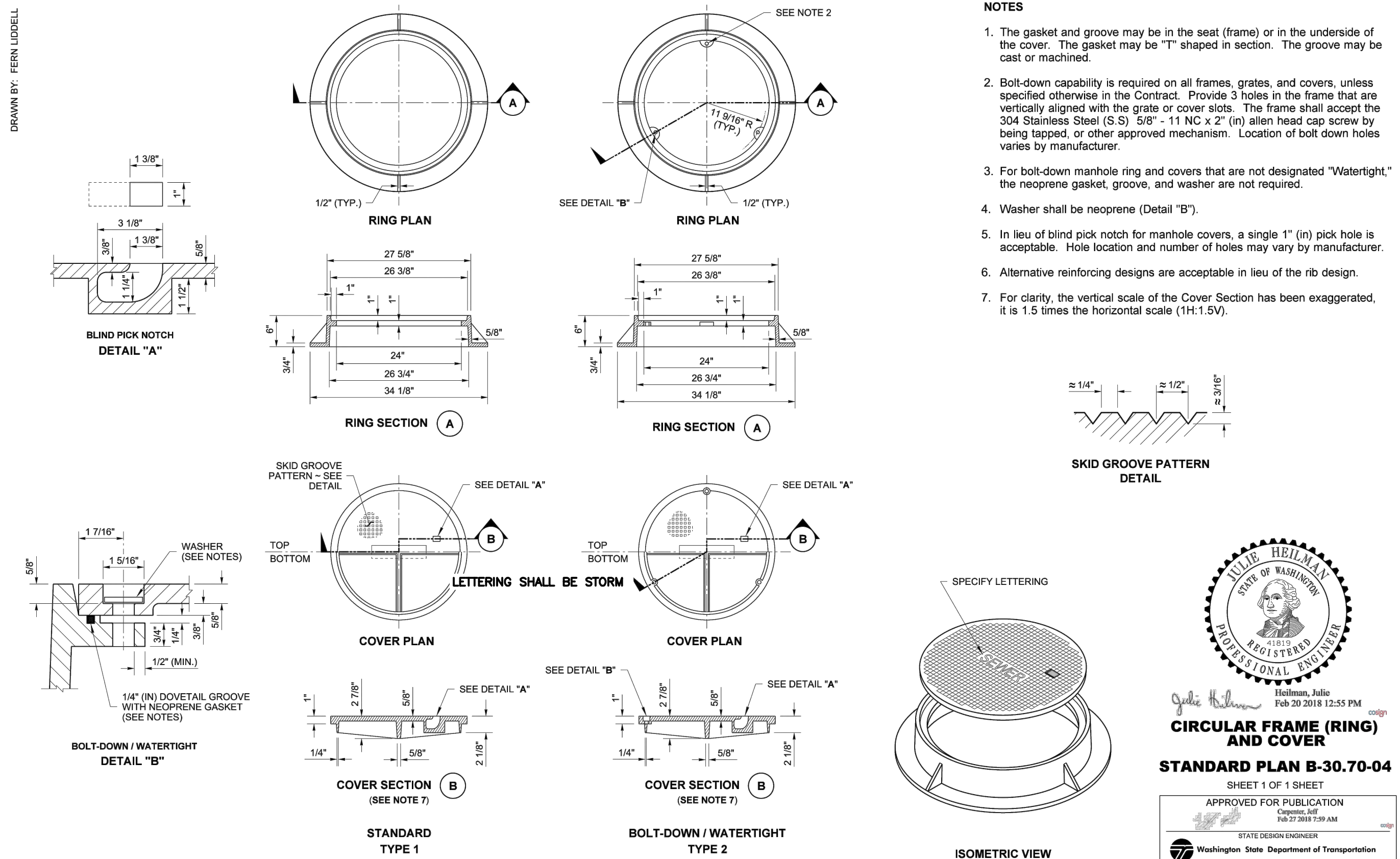
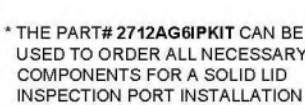
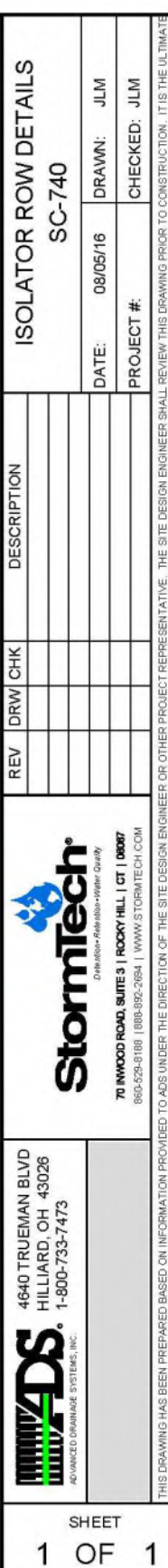
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**PACIFIC SURVEY
& ENGINEERING**

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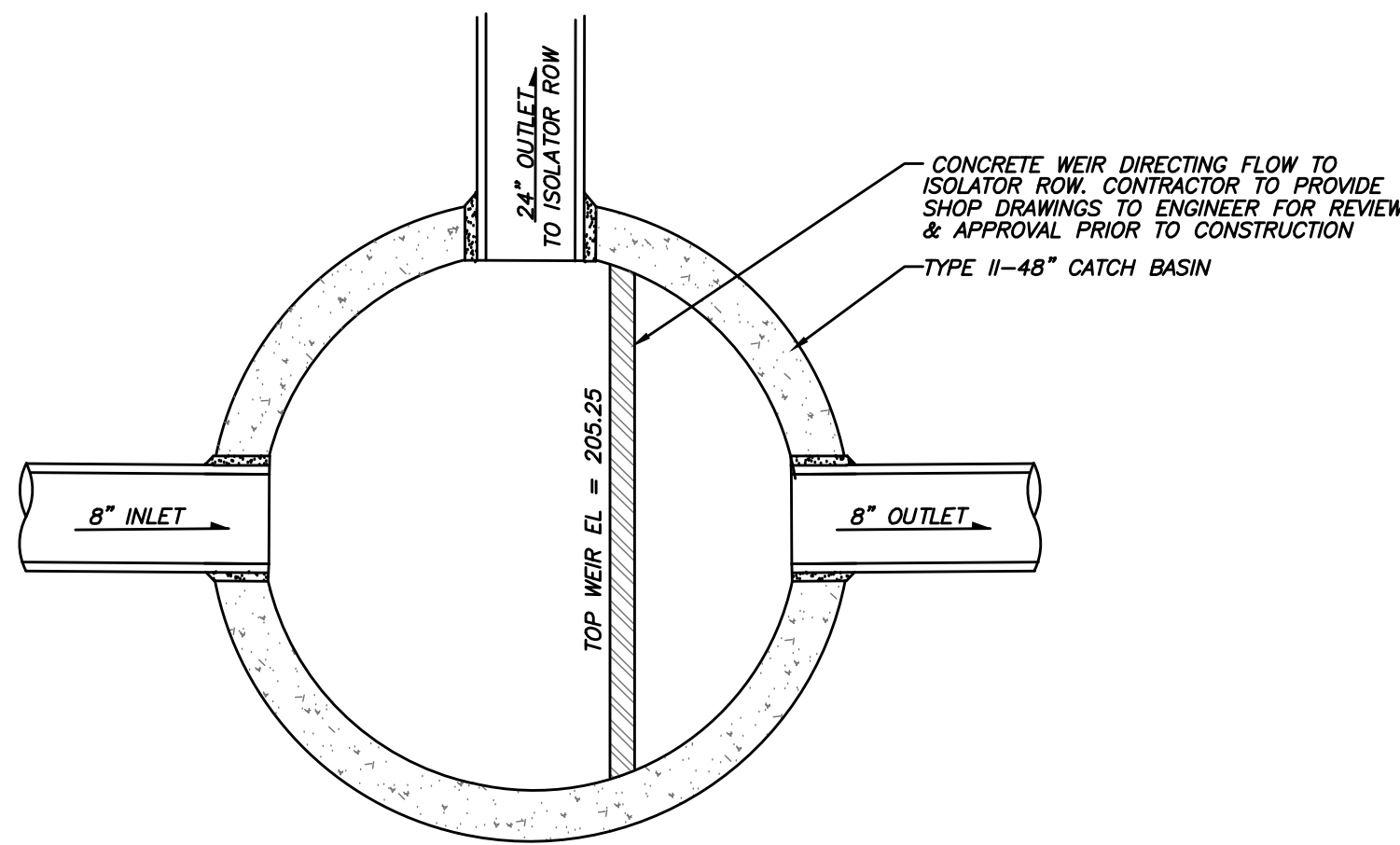
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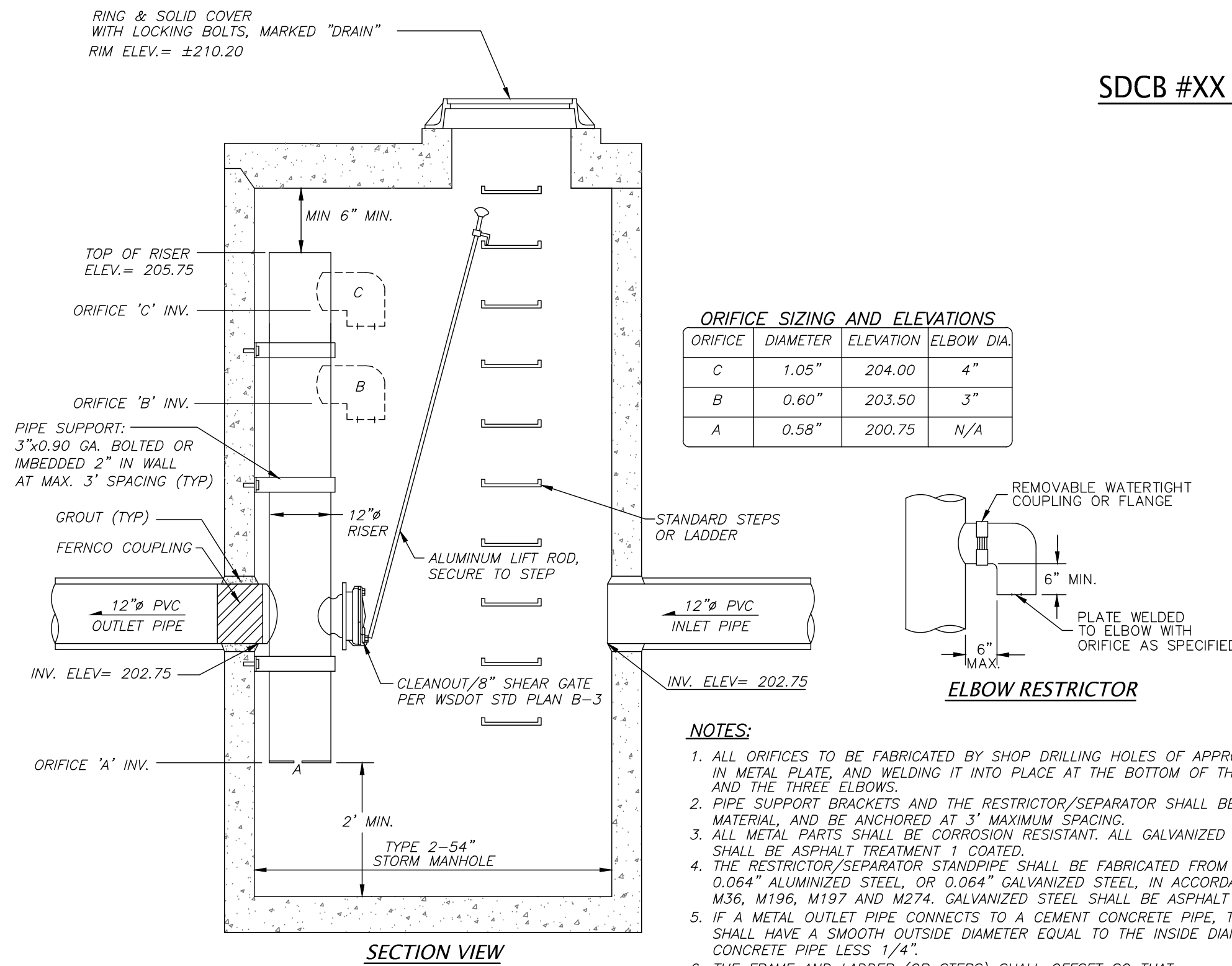
Job No: 2016112 Date: 17 MAY 2019
File No: 2016112 ecP SP DD
Drawn By: Josh Grimm
Checked By: Jeff Vander Yacht
Issued for: PERMIT DRAWINGS

DETAILS

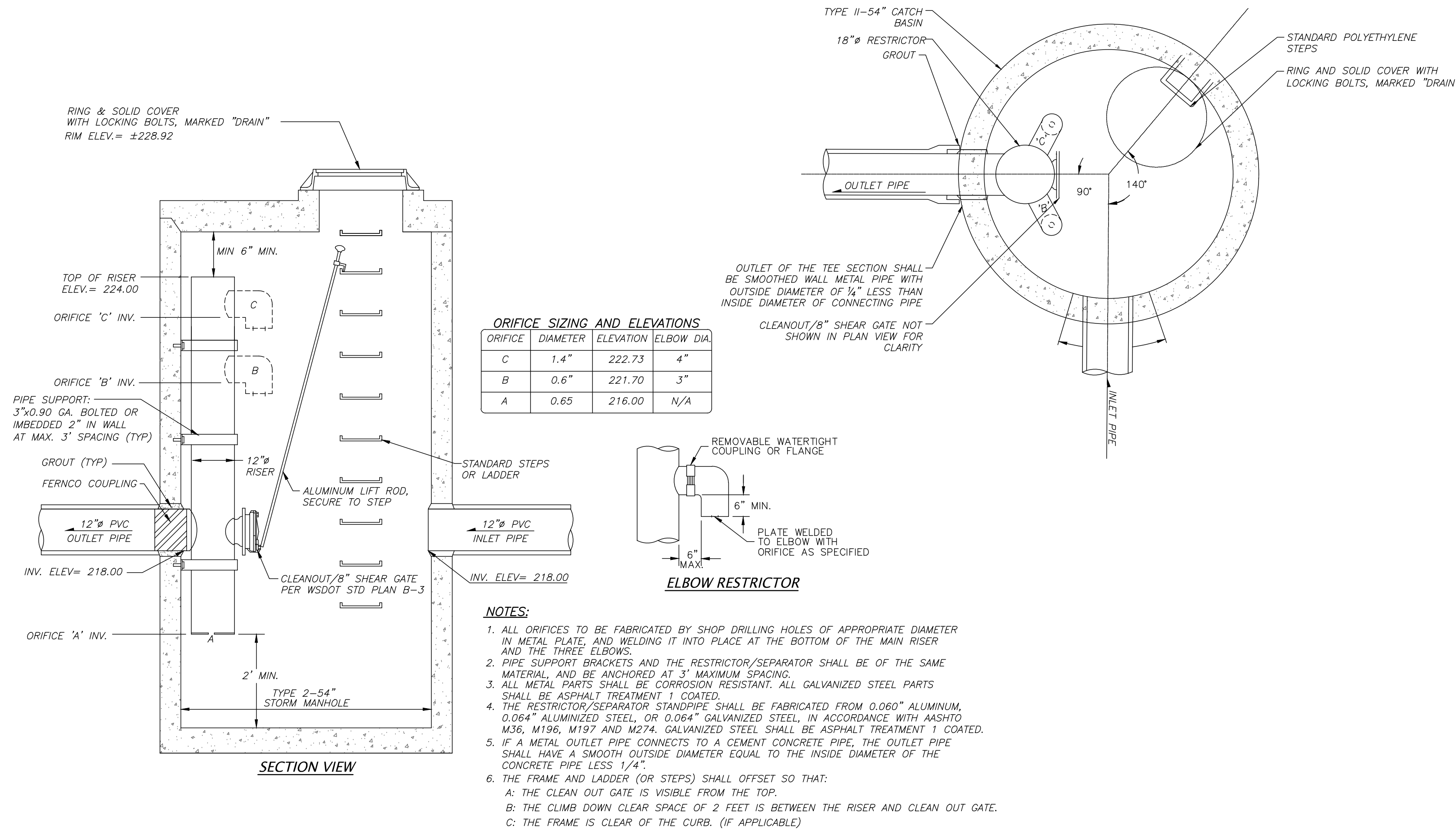
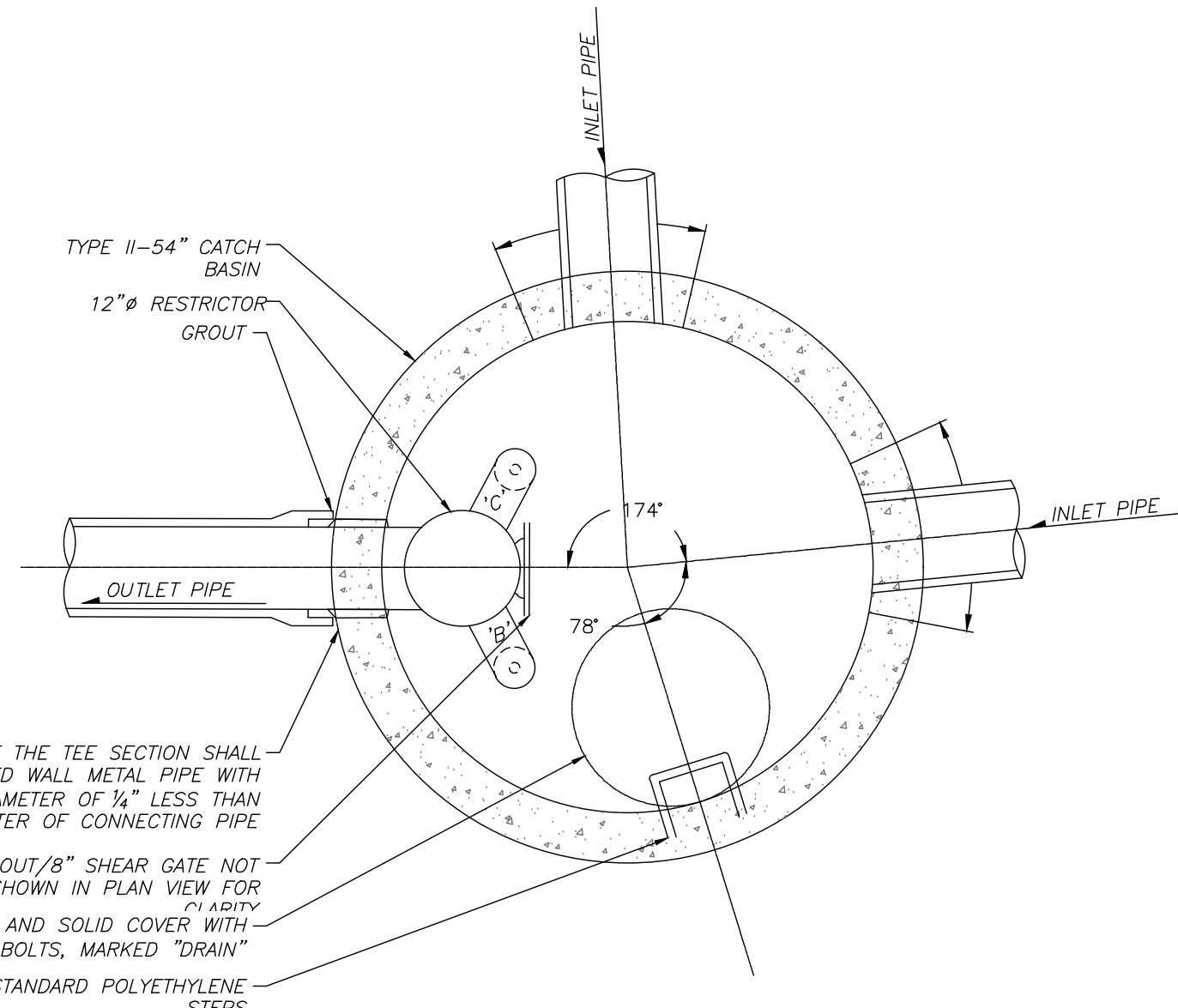
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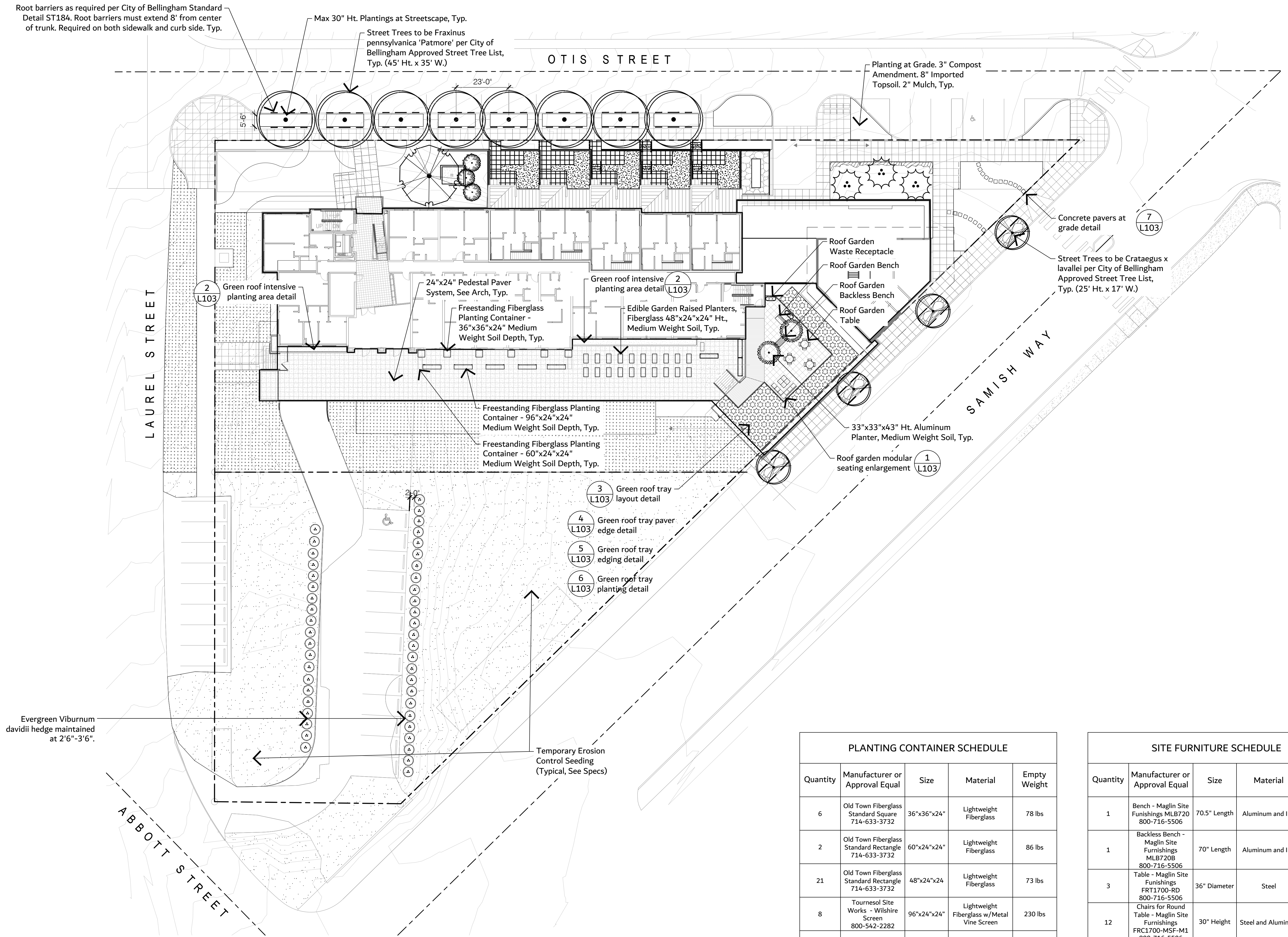


STORMTECH CONTROL STRUCTURE DETAIL
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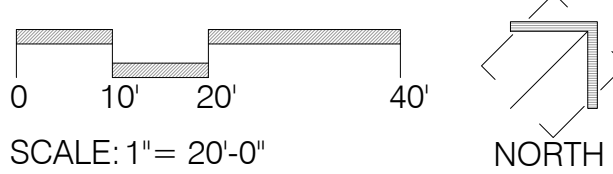
VAULT 'A' CONTROL STRUCTURE DETAIL
NOT TO SCALE





Planting Container Schedule				
Quantity	Manufacturer or Approval Equal	Size	Material	Empty Weight
6	Old Town Fiberglass Standard Square 714-633-3732	36"x36"x24"	Lightweight Fiberglass	78 lbs
2	Old Town Fiberglass Standard Rectangle 714-633-3732	60"x24"x24"	Lightweight Fiberglass	86 lbs
21	Old Town Fiberglass Standard Rectangle 714-633-3732	48"x24"x24"	Lightweight Fiberglass	73 lbs
8	Tournesol Site Works - Wilshire Screen 800-542-2282	96"x24"x24"	Lightweight Fiberglass w/Metal Vine Screen	230 lbs
2	Authentic Province Versailles Planter C 561-805-9995	33"x33"x43"	Aluminum	255 lbs

Site Furniture Schedule				
Quantity	Manufacturer or Approval Equal	Size	Material	Weight
1	Bench - Maglin Site Furnishings MLB720 800-716-5506	70.5" Length	Aluminum and IPE	115 lbs
1	Backless Bench - Maglin Site Furnishings MLB720B 800-716-5506	70" Length	Aluminum and IPE	75 lbs
3	Table - Maglin Site Furnishings FRT1700-RD 800-716-5506	36" Diameter	Steel	88 lbs
12	Chairs for Round Table - Maglin Site Furnishings FRCL700-MGF-M1 800-716-5506	30" Height	Steel and Aluminum	17 lbs
6	Outdoor Sectional - Maglin Site Furnishings Pixel Wood Style 2 Standard Back 800-716-5506	24"x24"	Steel and IPE	-
5	Outdoor Sectional Table - Maglin Site Furnishings Pixel Chroma Style 1 800-716-5506	24"x24"	Steel and Acrylic	-
1	Three Part Waste Bin - Maglin Site Furnishings MRC253 800-716-5506	54" Long	Steel	320 lbs

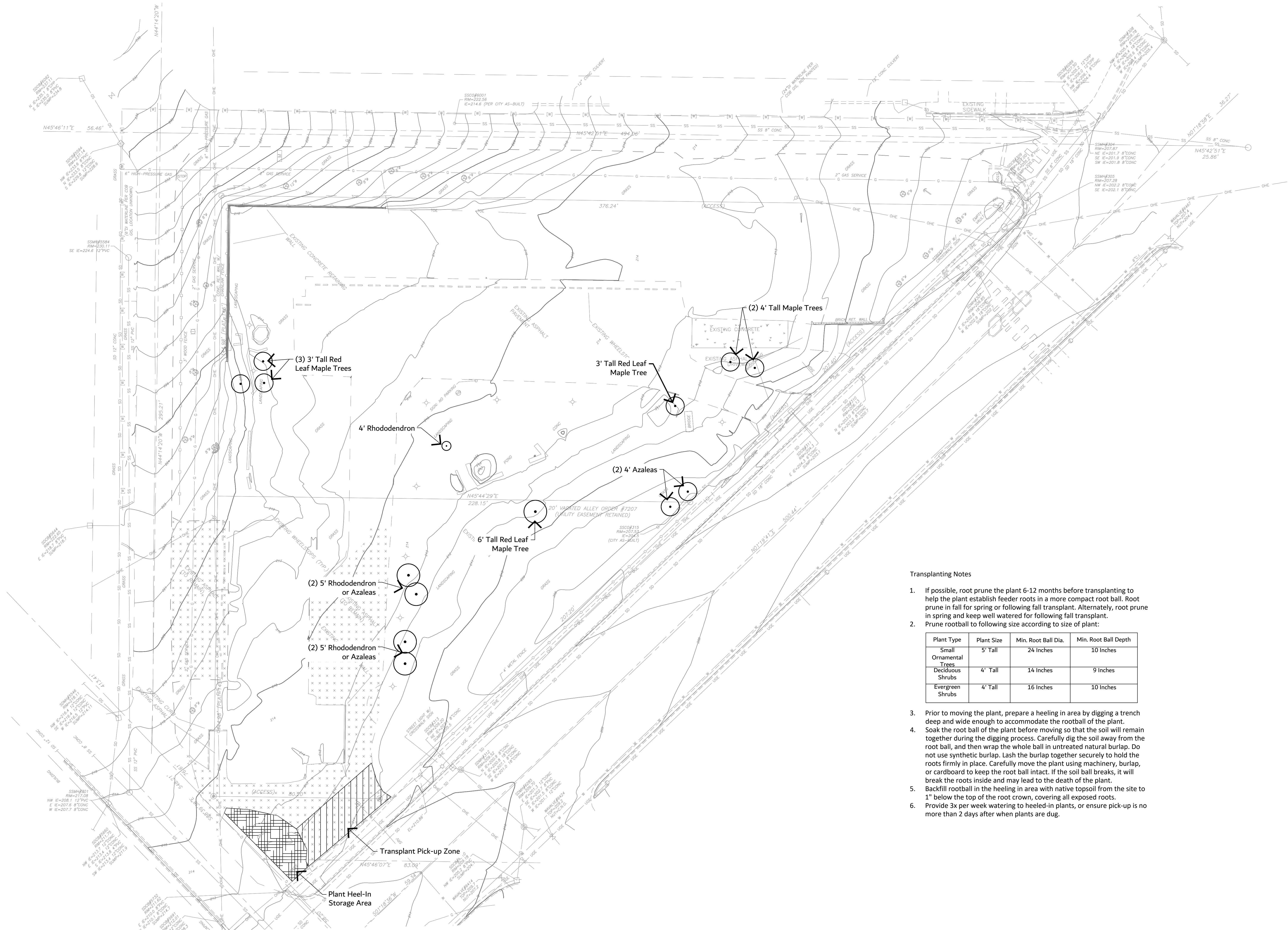


Samish Way Redevelopment (Aloha!) PHASE-I
315 North Samish Way
Samish Way Redevelopment Partners LLLP

Job No.: 1571.01 Date: 17 JUNE 2019
File No.: 1571.01 Phase 1 - BHA
Drawn By: PLA
Checked By: JFR
Issued for: CD MILESTONE #2

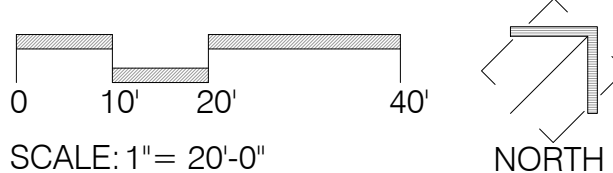
LANDSCAPE
REFERENCE
PLAN

L101



Transplanting Notes

1. If possible, root prune the plant 6-12 months before transplanting to help the plant establish feeder roots in a more compact root ball. Root prune in fall for spring or following fall transplant. Alternately, root prune in spring and keep well watered for following fall transplant.
 2. Prune rootball to following size according to size of plant:
- | Plant Type | Plant Size | Min. Root Ball Dia. | Min. Root Ball Depth |
|------------------------|------------|---------------------|----------------------|
| Small Ornamental Trees | 5' Tall | 24 inches | 10 inches |
| Deciduous Shrubs | 4' Tall | 14 inches | 9 inches |
| Evergreen Shrubs | 4' Tall | 16 inches | 10 inches |
3. Prior to moving the plant, prepare a heeling in area by digging a trench deep and wide enough to accommodate the rootball of the plant.
 4. Soak the root ball of the plant before moving so that the soil will remain together during the digging process. Carefully dig the soil away from the root ball, and then wrap the whole ball in untreated natural burlap. Do not use synthetic burlap. Lash the burlap together securely to hold the roots firmly in place. Carefully move the plant using machinery, burlap, or cardboard to keep the root ball intact. If the soil ball breaks, it will break the roots inside and may lead to the death of the plant.
 5. Backfill rootball in the heeling in area with native topsoil from the site to 1" below the top of the root crown, covering all exposed roots.
 6. Provide 3x per week watering to heeled-in plants, or ensure pick-up is no more than 2 days after when plants are dug.



Samish Way Redevelopment (Aloha!) PHASE-I
315 North Samish Way
Samish Way Redevelopment Partners LLLP

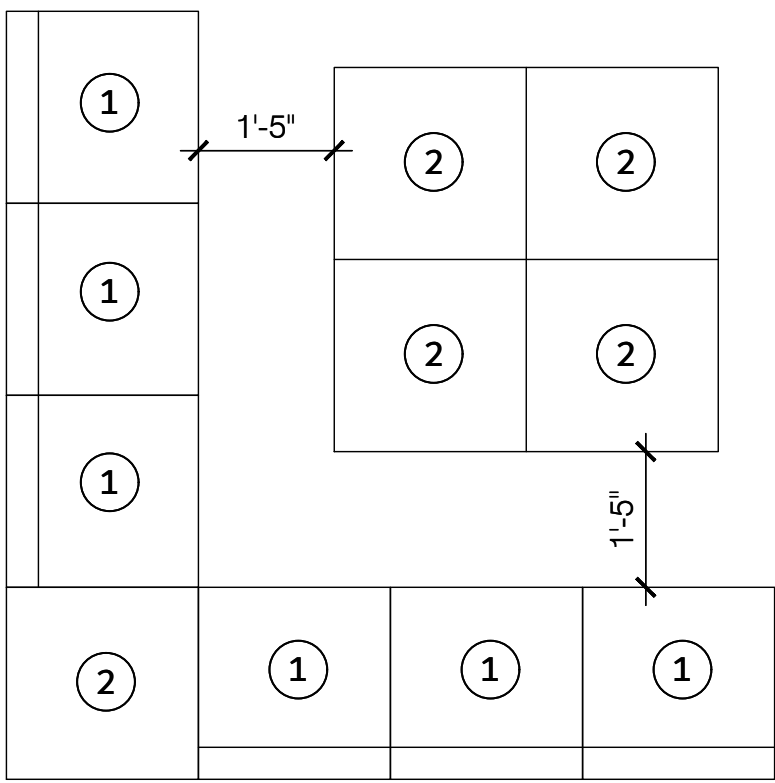
Job No.: 1571.01 Date: 17 JUNE 2019
File No.: 1571.01 Phase 1 - BHA
Drawn By: PLA
Checked By: JPR
Issued for: CD MILESTONE #2

LANDSCAPE
PLANT
SALVAGE
PLAN
L102



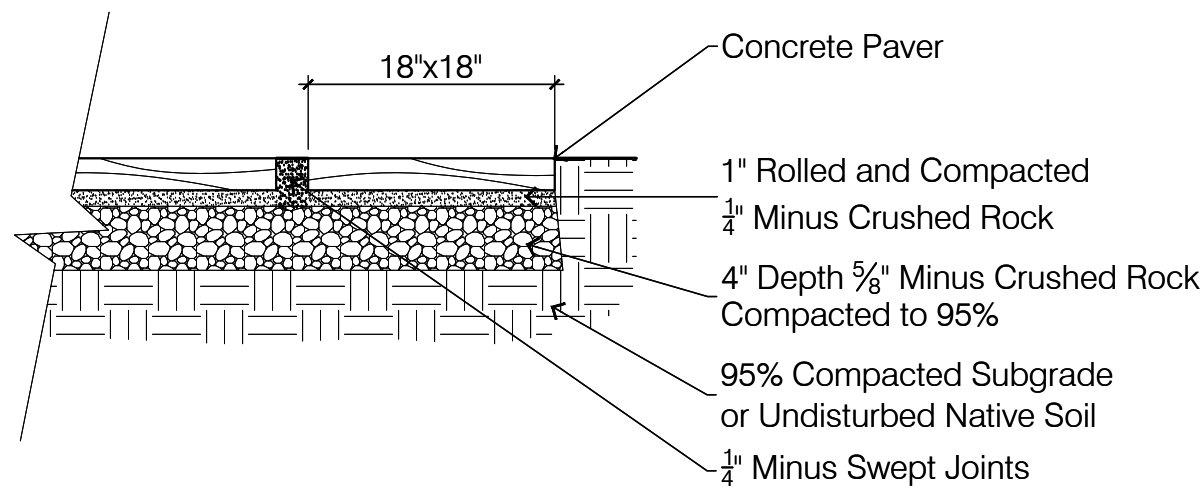
RMC Architects, PLLC 1223 Railroad Avenue Bellingham, WA 98225
P: 360.871.7123 F: 360.738.1444 rmc@rmcarchitects.com

- ① Maglin Site Furnishings Pixel Wood Style 2 Standard Back
- ② Maglin Site Furnishings Pixel Chroma Style 1



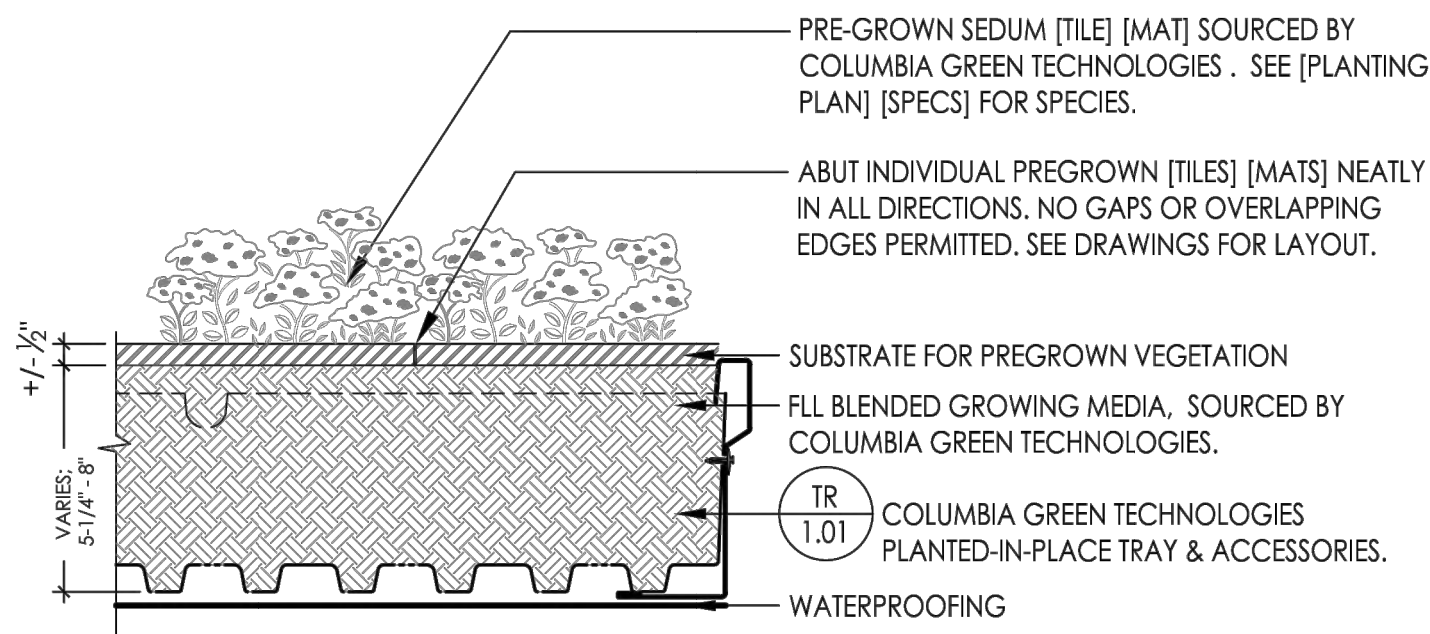
1 ROOF GARDEN MODULE SEATING ENLARGEMENT

SCALE: NTS



4 GREEN ROOF TRAY PAVER EDGE DETAIL

SCALE: NTS

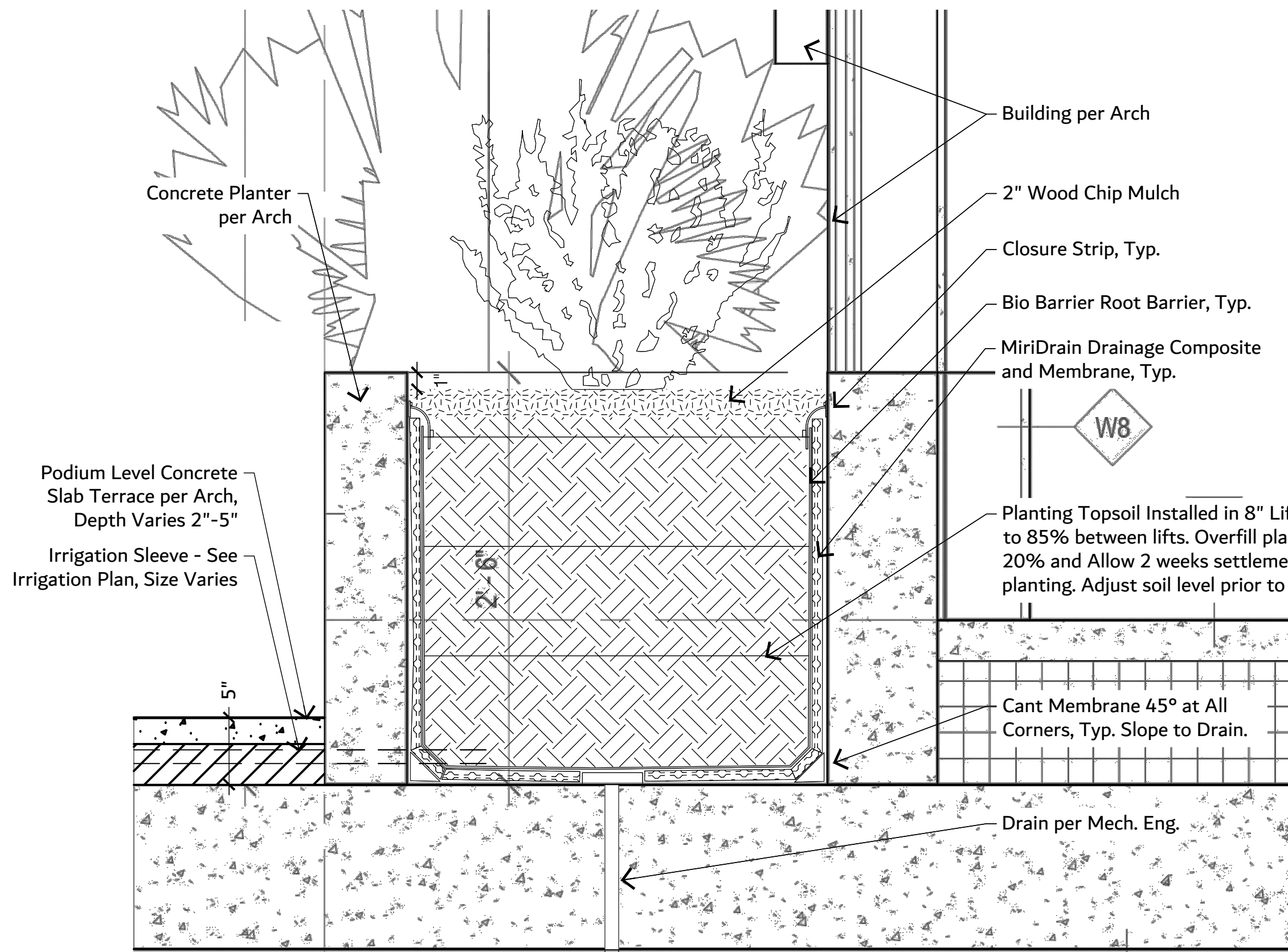


COLUMBIA GREEN TECHNOLOGIES	TR1.05	PLANTING METHOD: PREGROWN SEDUM TILE		COLUMBIA GREEN TECHNOLOGIES
		PLANTED-IN-PLACE TRAY	DETAIL SCALE: 3/4"=1'-0"	

Corporate Office: 79 SE Taylor Street, Suite 201 Portland, OR 97214
Telephone: (503) 327-8723 - www.columbia-green.com

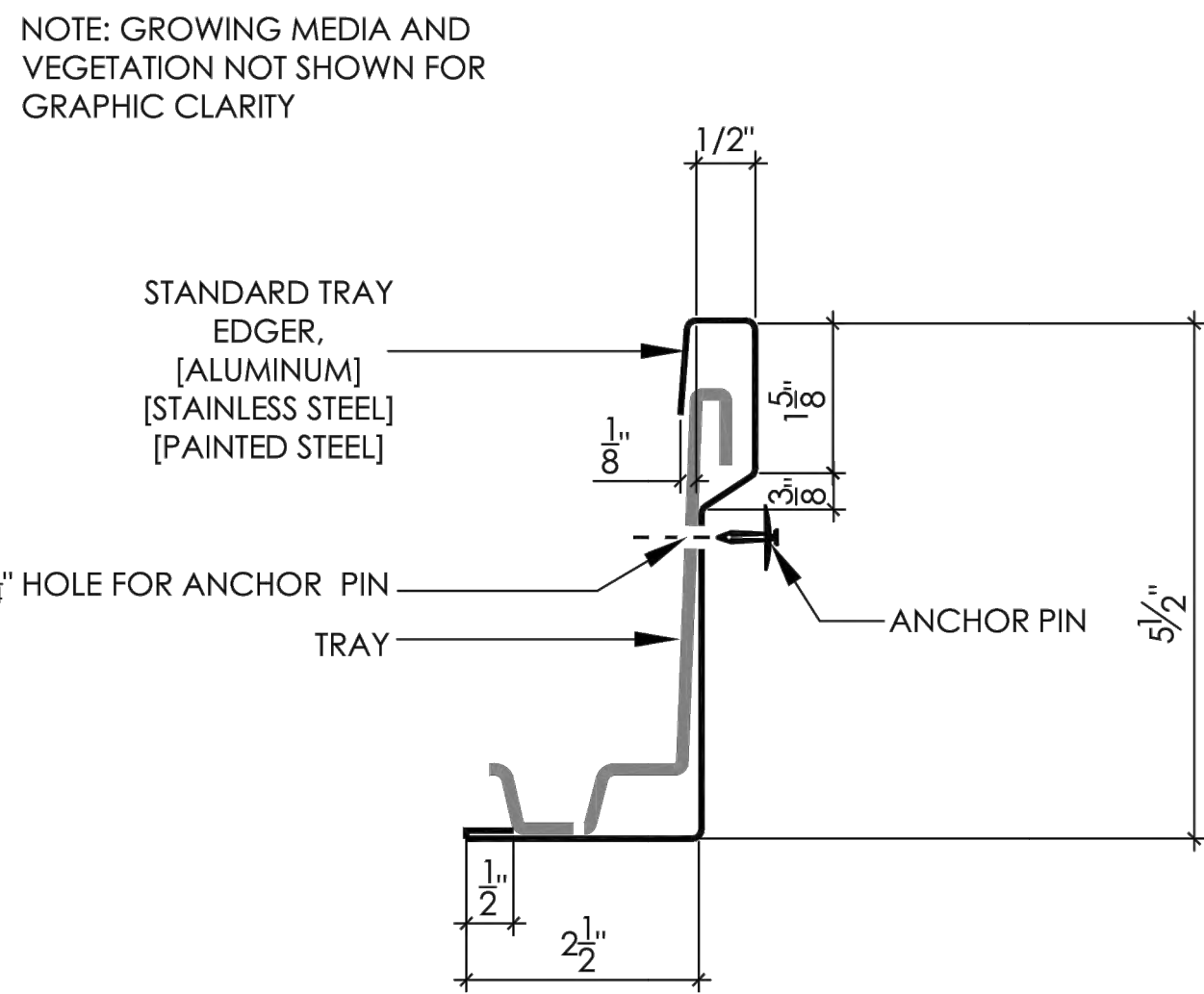
7 CONCRETE PAVERS AT GRADE DETAIL

SCALE: NTS



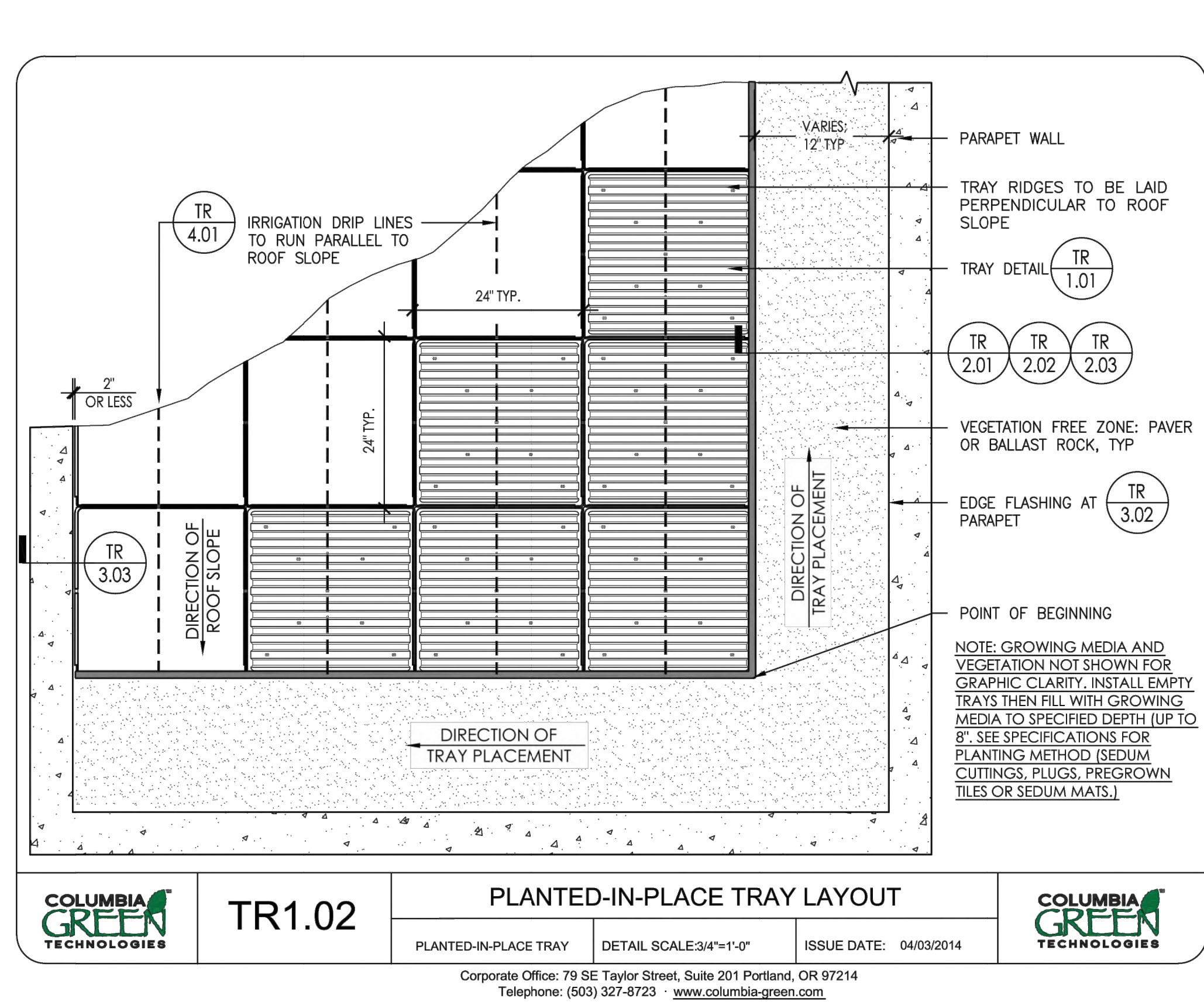
2 GREEN ROOF INTENSIVE PLANTING AREA DETAIL

SCALE: 1 1/2" = 1'-0"



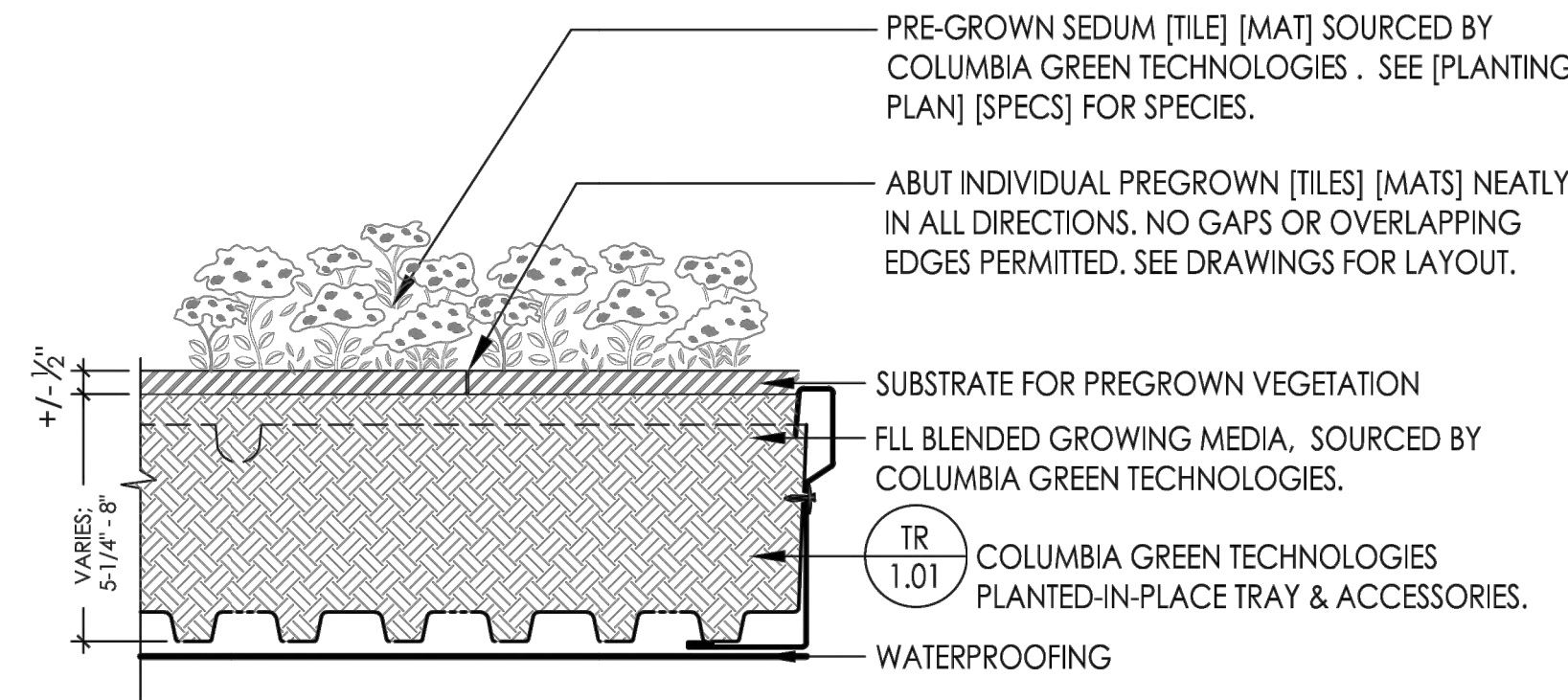
5 GREEN ROOF TRAY EDGING DETAIL

SCALE: NTS



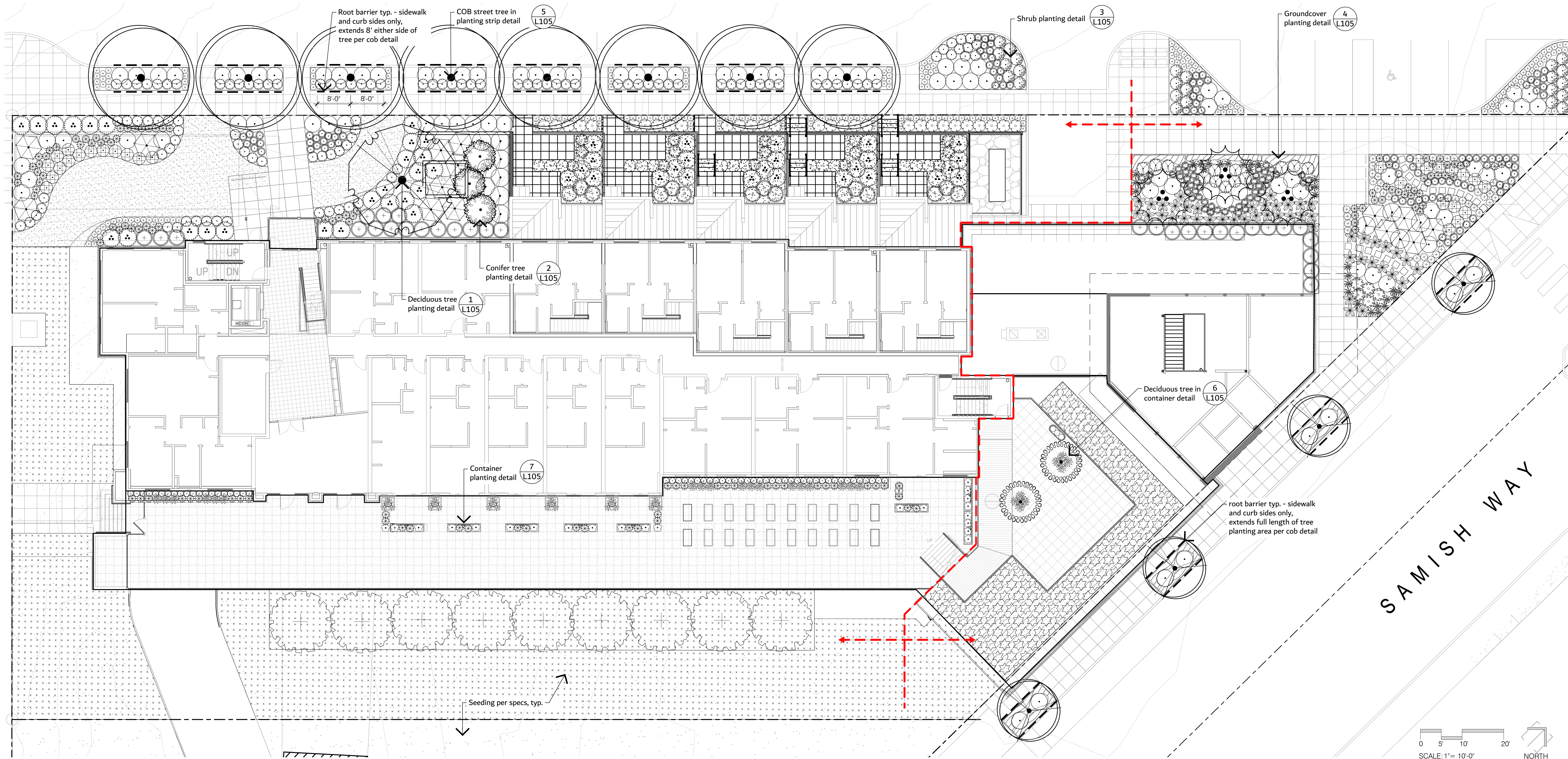
3 GREEN ROOF TRAY LAYOUT DETAIL

SCALE: NTS

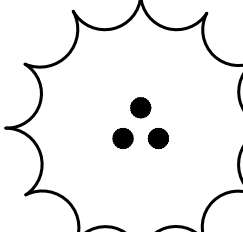
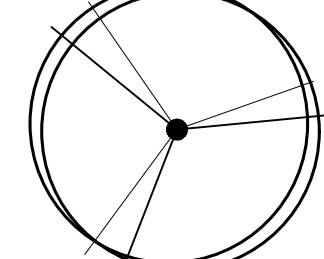
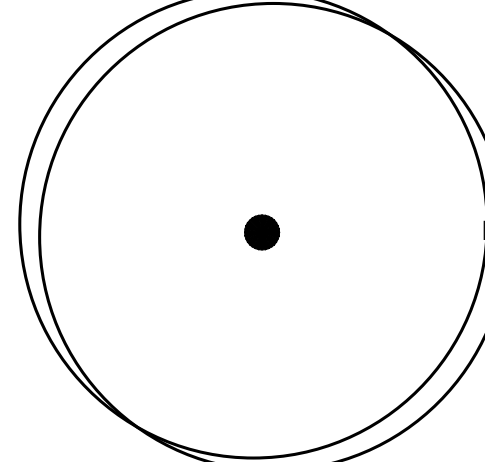
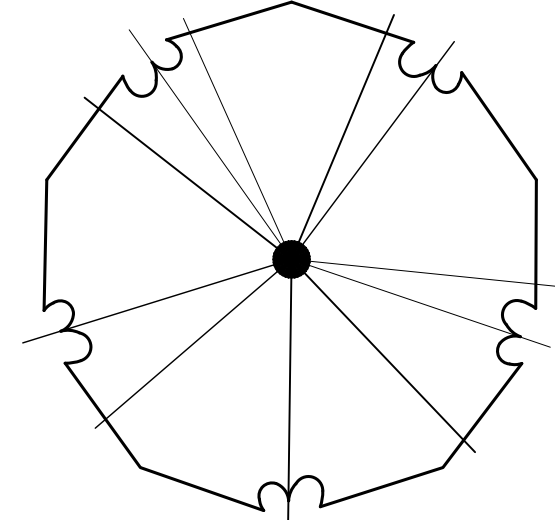


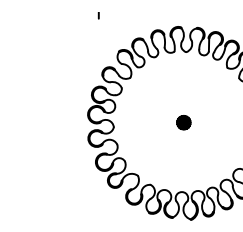
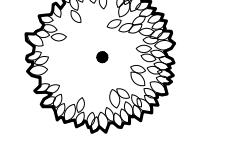


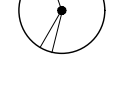

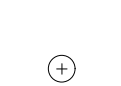


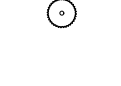
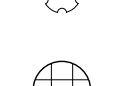



6 GREEN ROOF TRAY PLANTING DETAIL

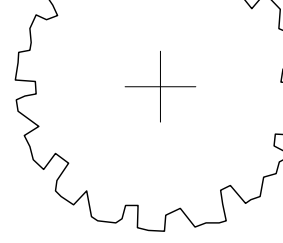










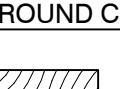


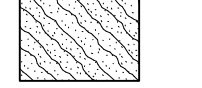
SCALE: NTS



PLANT SCHEDULE

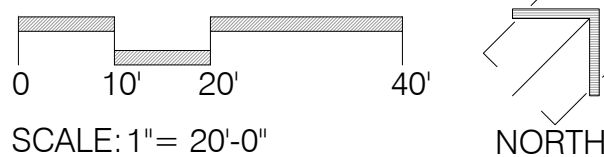
TREES	BOTANICAL NAME	COMMON NAME	SIZE	QTY	REMARKS
	Acer circinatum	Vine Maple	5 gal.	3	GF Small Tree, Native, DTAE
	Crataegus x lavallei	Hawthorn	2" Cal.	4	COB Street Tree List Small Selection, GF Med/Small Tree, DTAE
	Fraxinus pennsylvanica 'Patmore'	'Patmore' Ash	2" Cal.	8	COB Street Tree List Small/Med Selection, GF Med/Large Tree, DTAE
	Ginkgo biloba	Maidenhair Tree	2" Cal.	1	GF Med/Lg Tree, DTAE

SHRUBS	BOTANICAL NAME	COMMON NAME	SIZE	QTY	REMARKS
	Parrotia persica	Persian Parrotia	1" Cal.	2	GF Small Tree, DTAE
	Picea omorika 'Bruns'	Bruns Spruce	4' Ht.	3	Medium Tree, DTAE
	Abelia x 'Rose Creek'	Rose Creek Abelia	1 gal.	45	GF Plant List, DTAE, +24"
	Blechnum spicant	Deer Fern	1 gal.	29	GF Plant List, DTAE
	Cistus x hybridus	White Rockrose	2 gal.	12	GF Plant List, DTAE, +24"
	Clematis armandii	Evergreen Clematis	1 gal.	8	GF Plant List, DTAE, +24"
	Cornus sericea 'Kelsey'	Kelsey Dogwood	2 gal.	110	GF Plant List, DTAE, +24"
	Geranium macrorrhizum 'Ingwersen's Variety'	Ingwersen's Variety Geranium	1 gal.	257	GF Plant List, DTAE
	Helianthemum nummularium 'Henfield Brilliant'	Henfield Brilliant Rock Rose	1 gal.	24	DTAE
	Helictotrichon sempervirens 'Sapphire'	Blue Oat Grass	1 gal.	35	Great Plant Pick, DTAE, +24"
	Ilex crenata 'Mariesii'	Japanese Holly	2 gal.	56	GF Plant List Variety, DTAE, +24"
	Lavandula stoechas 'Winter Bee'	Spanish Lavender	1 gal.	23	GF Plant List, DTAE, +24"
	Lavandula x intermedia 'Phenomenal'	Lavender	1 gal.	28	GF Plant List, DTAE, +24"
	Mahonia nervosa	Oregon Grape	1 gal.	51	GF Plant List, Native, DTAE, +24"

GROUND COVERS	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	QTY	REMARKS
	Morella californica	California Wax Myrtle	5 gal.	9	GF Plant List, Native, DTAE, +24"	
	Nepeta racemosa 'Walker's Low'	Catmint	1 gal.	32	GF Plant List, DTAE, +24"	
	Perovskia x 'Little Spire'	Russian Sage	2 gal.	123	DTAE, +24"	
	Polystichum munitum	Western Sword Fern	1 gal.	47	GF Plant List, Native, DTAE, +24"	
	Prunus laurocerasus 'Mount Vernon'	Mount Vernon Laurel	2 gal.	92	GF Plant List, DTAE, +24"	
	Rhododendron azalea 'Tenino'	Tenino Azalea	2 gal.	13	GF Plant List, DTAE, +24"	
	Rhododendron x 'Golden Gate'	Golden Gate Rhododendron	5 gal.	20	GF Plant List, DTAE, +24"	
	Ribes sanguineum 'King Edward VII'	Red Flowering Currant	2 gal.	14	GF Plant List, DTAE, +24"	
	Sedum album 'Coral Carpet'	Coral Carpet Stonecrop	1 gal.	12	GF Plant List Variety, DTAE	
	Sesleria autumnalis	Autumn Moor Grass	1 gal.	105	DTAE, +24"	
	Vaccinium ovatum	Evergreen Huckleberry	2 gal.	46	GF Plant List, Native, DTAE, +24"	
	Viburnum davidii	David Viburnum	2 gal.	69	GF Plant List, DTAE, +24"	
	Epimedium x rubrum	Red Barrenwort	1 gal.	24" o.c.	56	GF Plant List, DTAE
	Festuca arundinacea	Tall Fescue	seed		1,183 sf	
	Sedum oregonum	Oregon Sedum	flat		1,061 sf	Sedum Tile or Tray, GF Plant List, DTAE

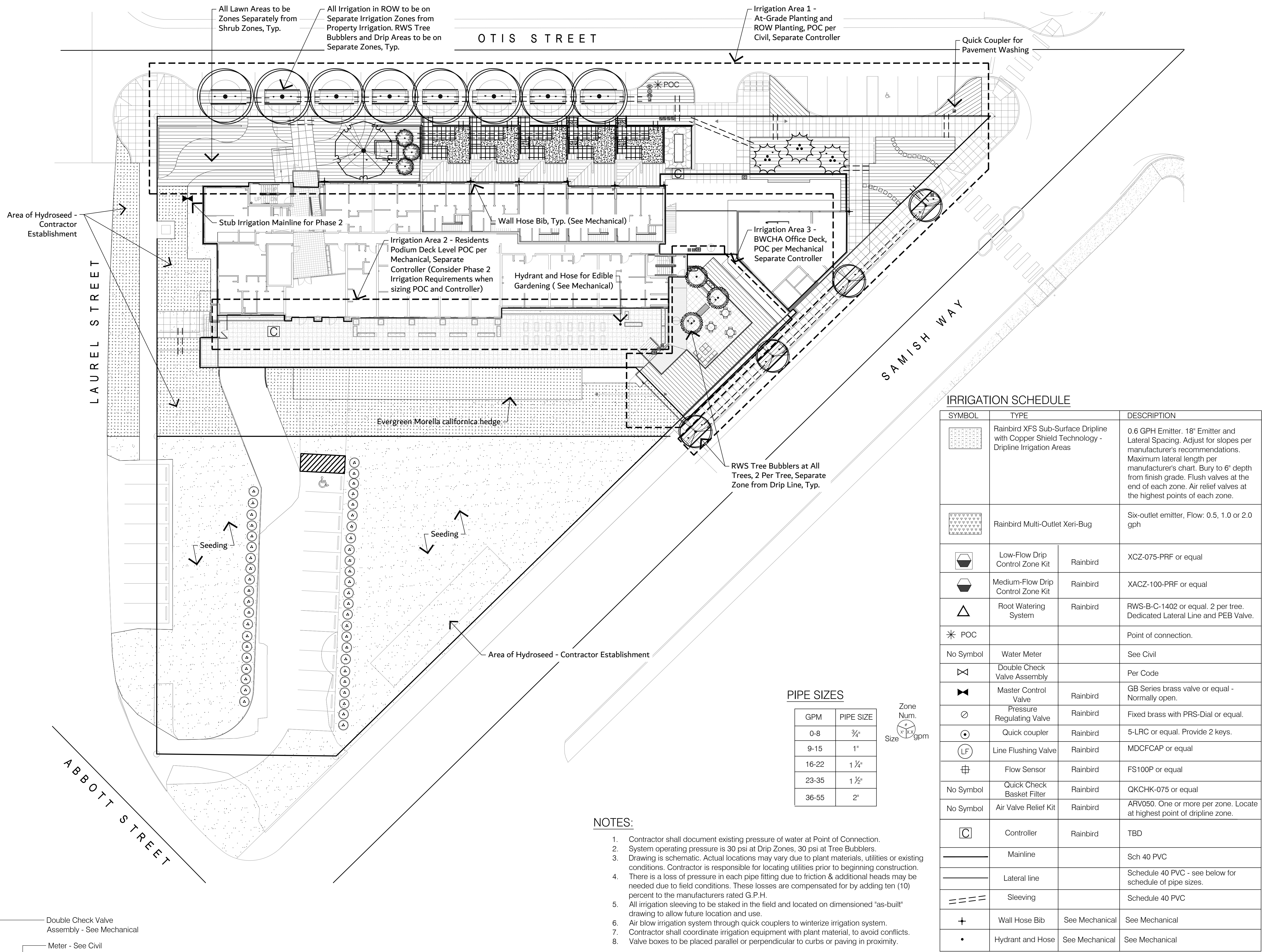
GF = Green Factor
+24" = Taller than 24" at Maturity
DTAE = Drought Tolerant After Establishment

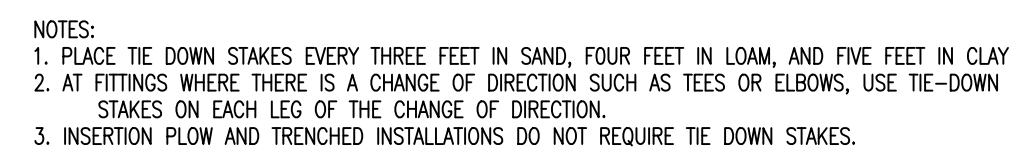
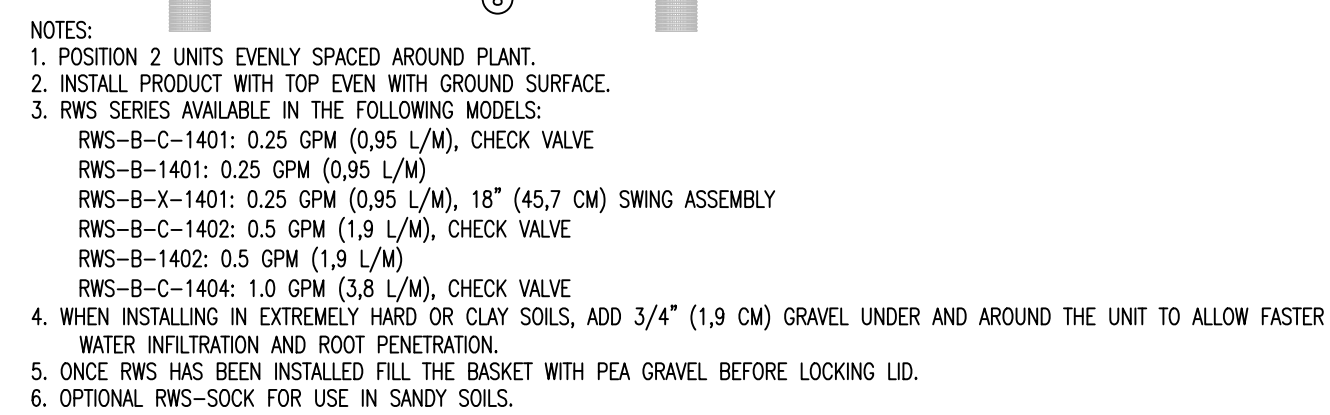
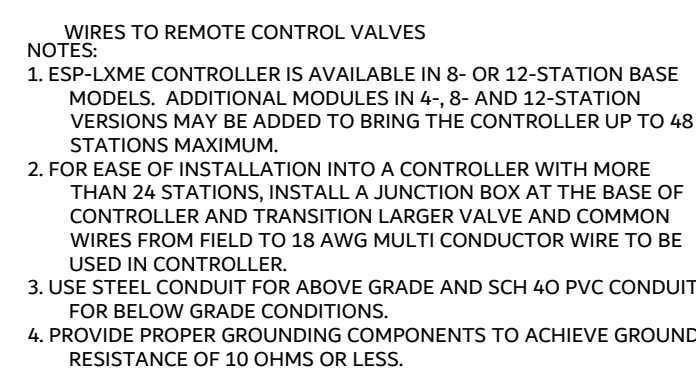
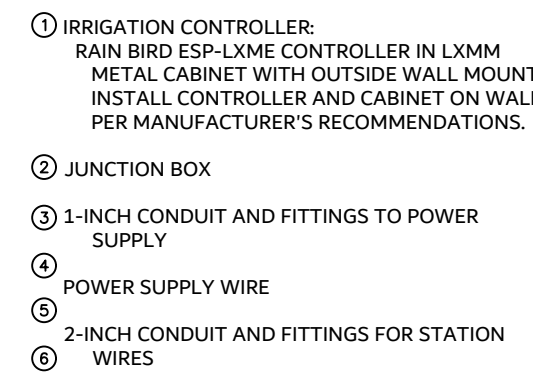


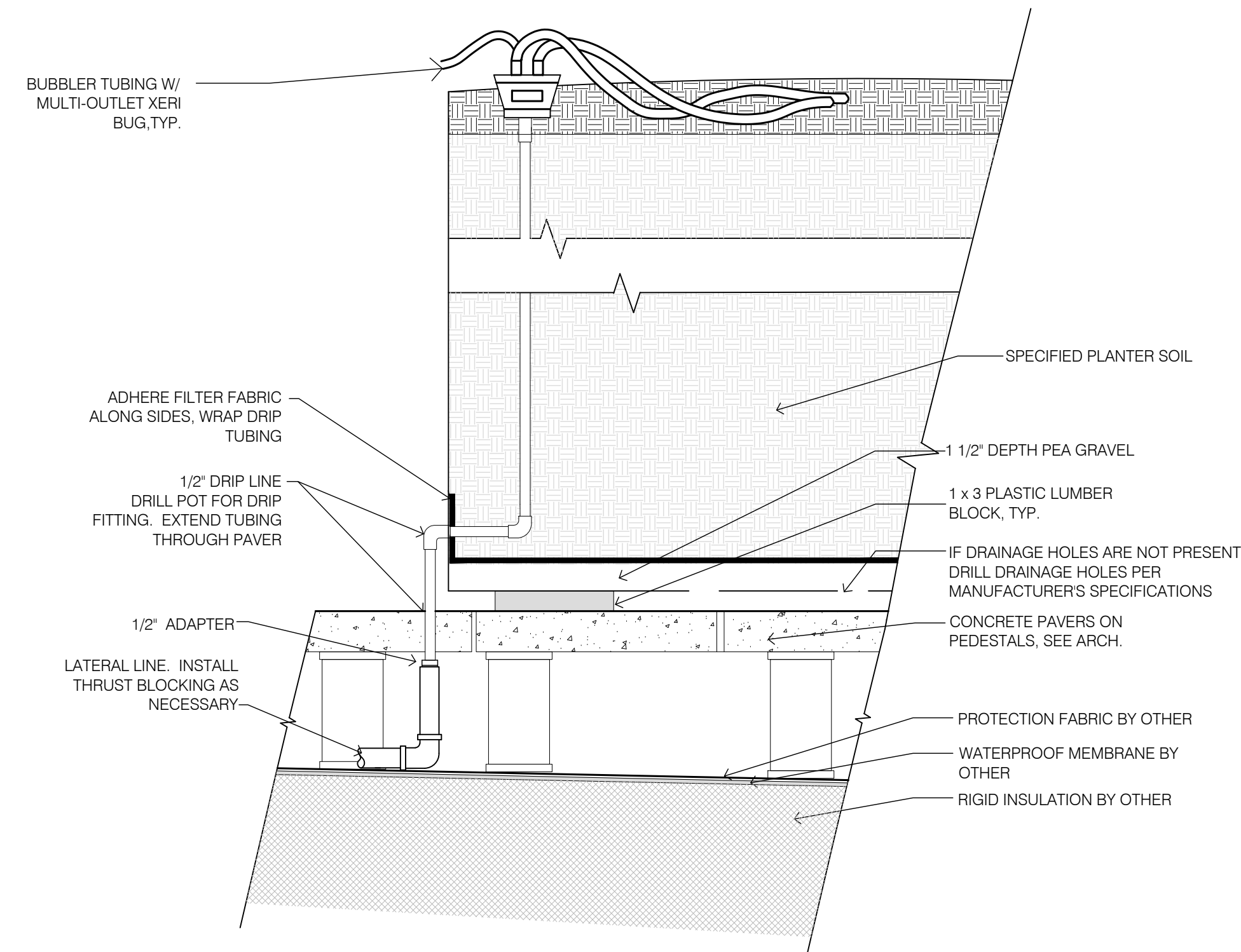


Green Factor Worksheet*																
		Planting Area													TOTAL**	
		1	2	3	4	5	6	7	8	9	10	11	12	13		14
A1	square foot															0
A2	square foot	802	706	3361	3213	1961	283				8001					17718
A3	square foot															0
B1	square foot	802	706	3351	3232	1961	283				8001					17719
B2	square foot	176	190	595	626	211	358	6	2	150	18	0				1300
B3	square foot															0
B4	# of trees							3	4							7
B5	square foot															0
B6	# of trees	3		1												5
B7	# of trees															0
C1	square foot								1073	283	208					1561
C2	square foot															0
D	square foot															0
E1	square foot															0
E2	square foot															0
F2	square foot															0
G	square foot															0
H1	square foot	802	706		3213	1961	283	706	161	208						8845
H2	square foot															0
I	square foot															0
J	square foot								122							122

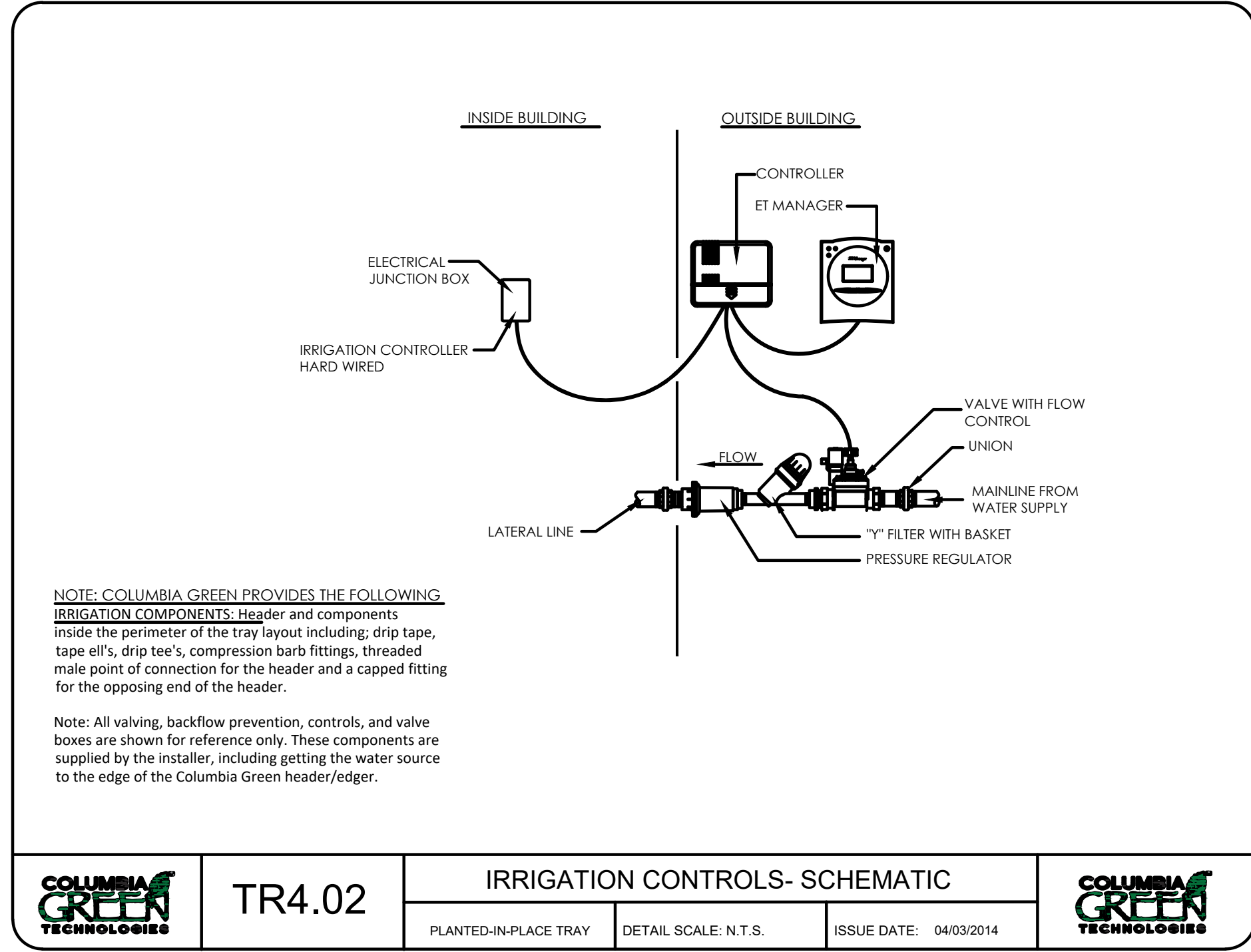
* See Green Factor score sheet for category definitions
 ** Total based on the Green Factor score sheet



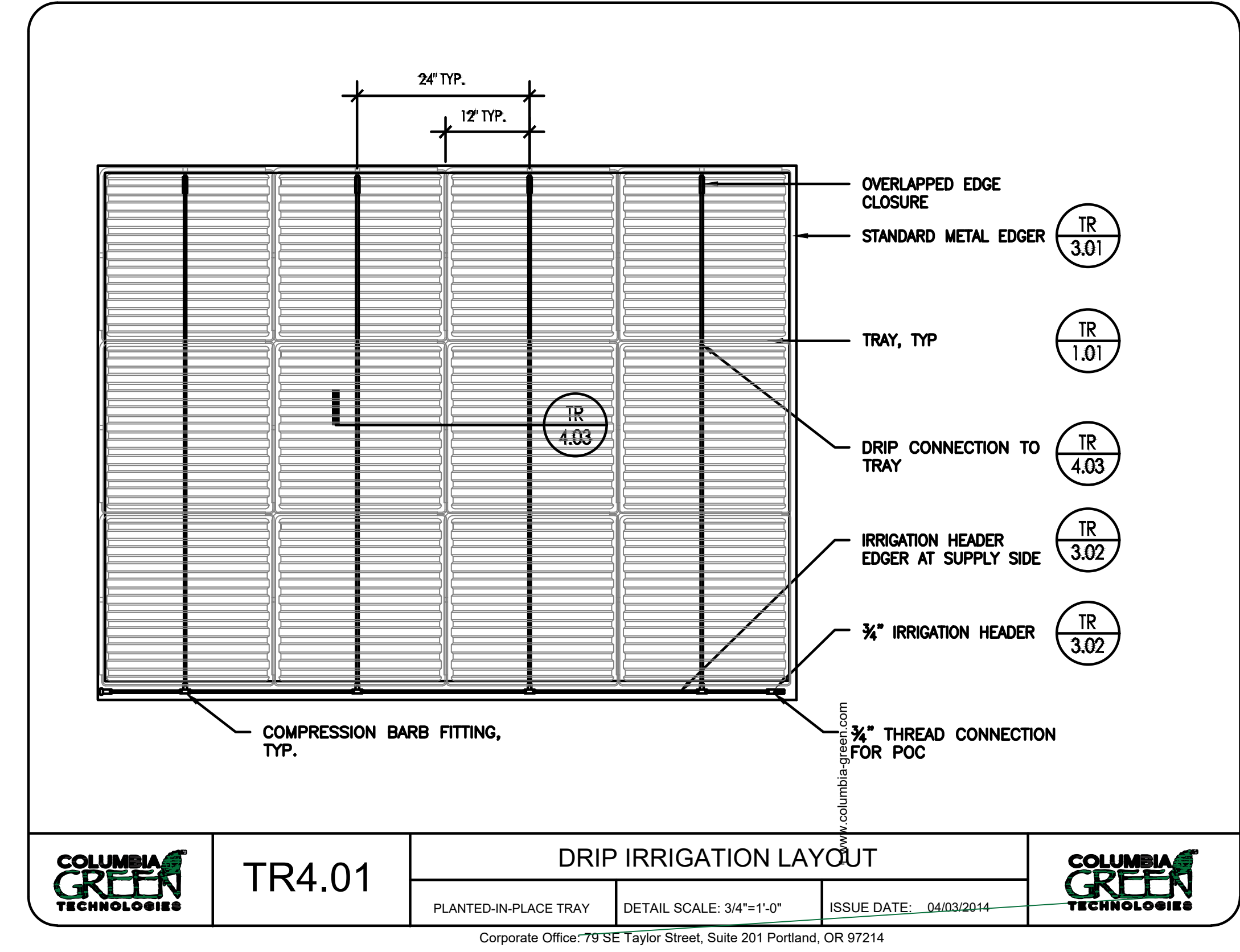




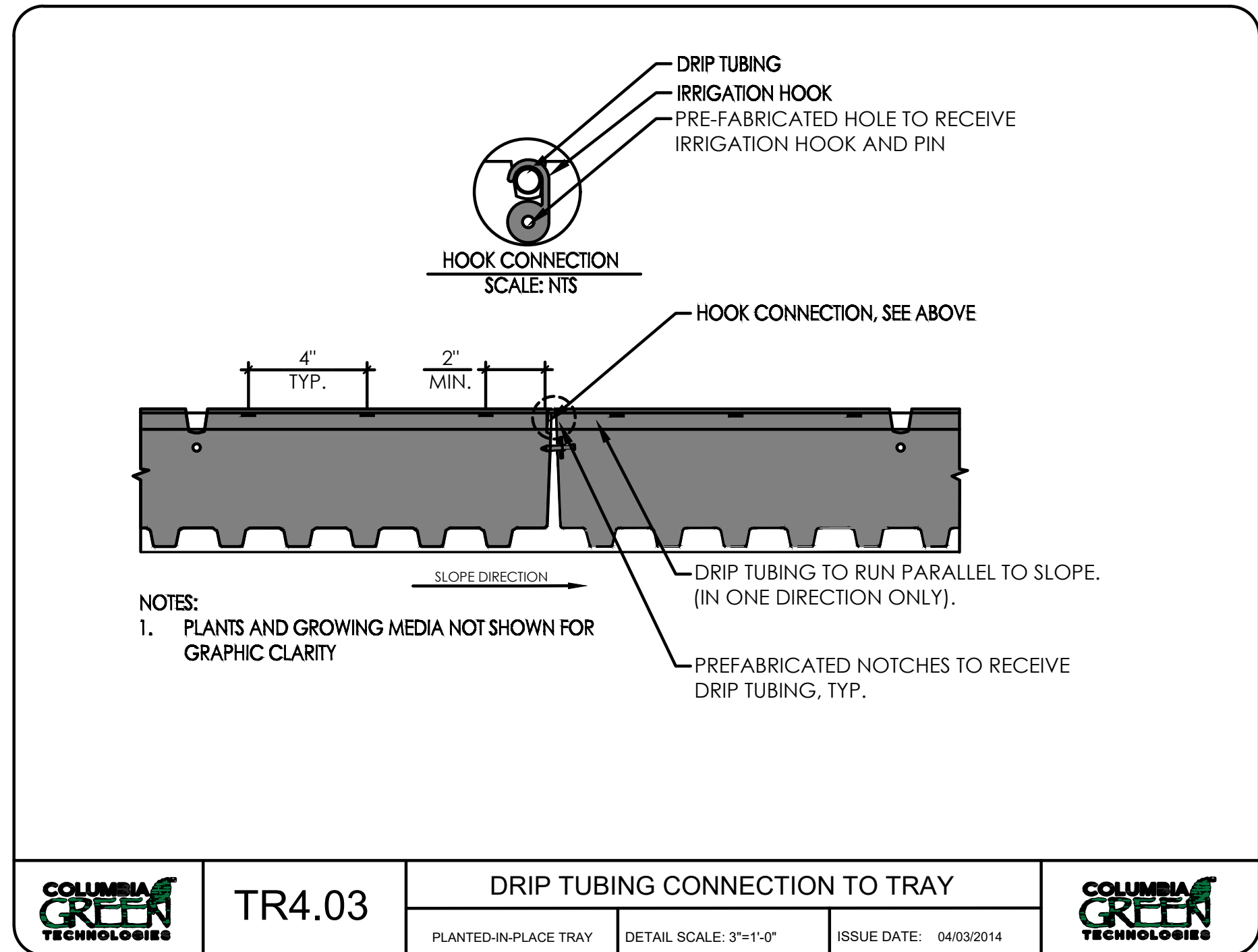
1 ROOF GARDEN CONTAINER IRRIGATION DETAIL
SCALE: NTS



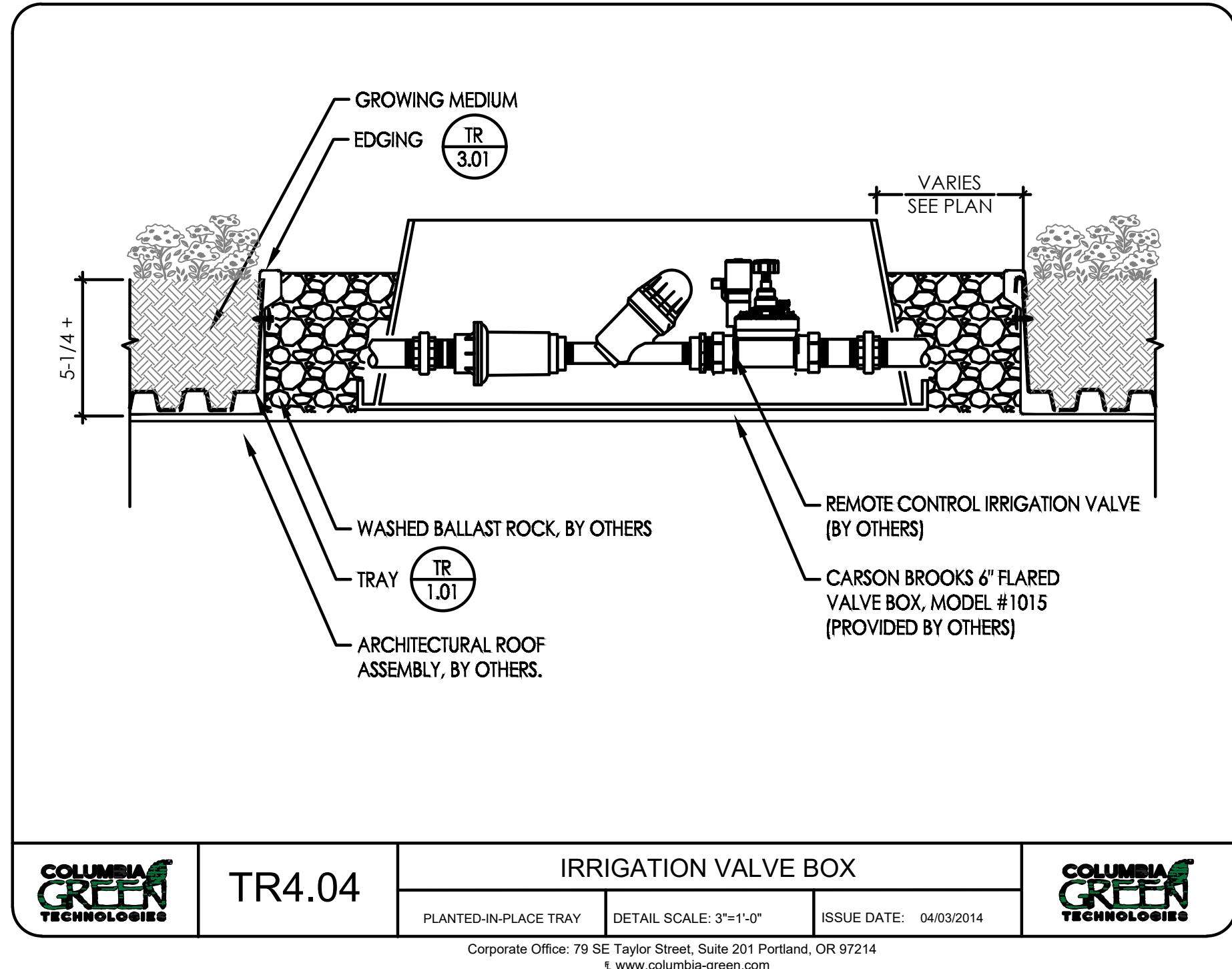
2 GREEN ROOF TRAY IRRIGATION CONTROLS DETAIL
SCALE: NTS



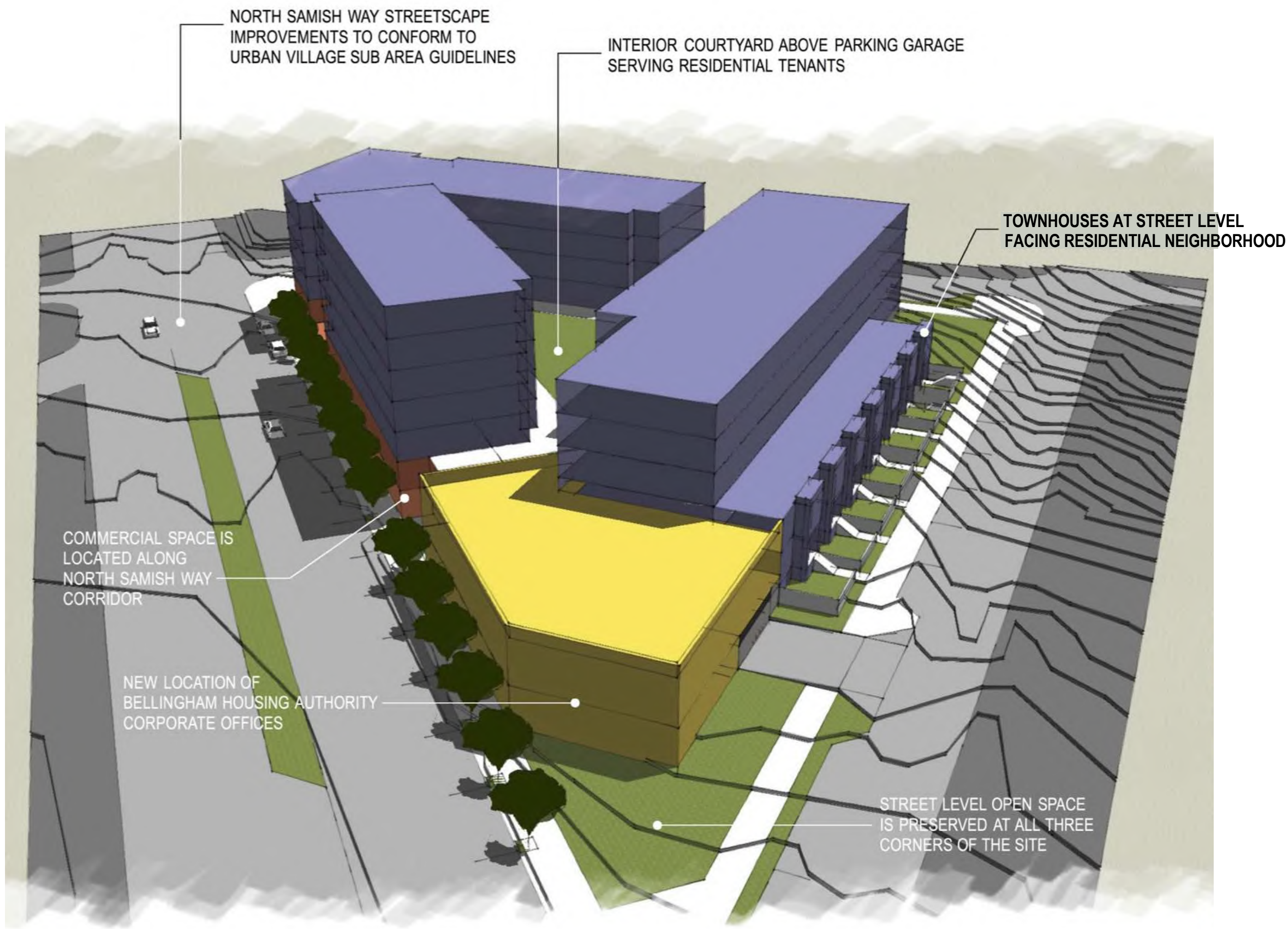
3 GREEN ROOF TRAY IRRIGATION LAYOUT DETAIL
SCALE: NTS



4 GREEN ROOF TRAY IRRIGATION DRIP TUBE DETAIL
SCALE: NTS



5 GREEN ROOF TRAY IRRIGATION DRIP TUBE DETAIL
SCALE: NTS



Massing Study



Vicinity Plan

Samish Way Redevelopment

Bellingham Whatcom County Housing Authority

Project Info

OWNER:	BELLINGHAM WHATCOM COUNTY HOUSING AUTHORITY
SITE ADDRESS'S:	301 & 315 NORTH SAMISH WAY, BELLINGHAM, WA 98225
PARCEL NO'S.:	380331425243 & 380331421256
LEGAL DESC.:	<p>LOTS 10,11,12,13,14,15,16,17,18,19, AND THAT PORTION OF LOT 20, LYING WESTERLY OF STATE ROAD NO.1 (PACIFIC HIGHWAY), BLOCK 26, "MAP OF ELDRIDGE AND BARTLETT'S ADDITION TO THE TOWN OF SCHOWE, WHATCOM COUNTY, W.T.", NOW A PART OF THE CONSOLIDATED CITY OF BELLINGHAM, WHATCOM COUNTY, WASHINGTON, ACCORDING TO THE PLAT THEREOF, RECORDED IN BOOK 1 OF PLATS, PAGE 91, IN THE AUDITOR'S OFFICE OF SAID COUNTY AND STATE.</p> <p>TOGETHER WITH THE VACATED ALLEY, VACATED NORTHEAST TEN FEET OF LAUREL STREET, VACATED NORTHEAST TEN FEET OF PASCO STREET, AND THE VACATED SOUTHEAST TEN FEET OF OTIS STREET, ALL ABUTTING THEREOF.</p> <p>EXCEPT THAT PORTION OBTAINED BY THE STATE OF WASHINGTON AND CONVEYED TO THE STATE OF WASHINGTON FOR STATE ROAD NO.1 OR PACIFIC HIGHWAY, BY:</p> <p>A. DECREE OF CONDEMNATION IN THE SUPERIOR COURT OF THE STATE OF WASHINGTON UNDER CLAUSE NO.21361</p> <p>B. STATE DEED RECORDED OCTOBER 17, 1931, IN VOLUME 220 OF DEEDS, PAGE 30, UNDER AUDITOR'S FILE NO.403154.</p> <p>C. STATE DEED RECORDED DECEMBER 9,1931, IN VOLUME 219 OF DEEDS, PAGE 337, UNDER AUDITOR'S FILE NO.403814.</p> <p>SITUATE IN WHATCOM COUNTY, WASHINGTON.</p>
SITE AREAS:	22,151.0339 S.F. & 43,754.5665 S.F. (TOTAL: 65,905.599 S.F.)

Drawing Index

MP000	COVER SHEET
MP100	SITE PLAN / P1 LEVEL
MP101	SITE PLAN / P2 LEVEL
MP200	PODIUM LEVEL PLAN
MP201	TYPICAL UPPER LEVELS 2, 3, 4
MP300	CONCEPT ELEVATIONS
MP302	SITE CONTEXT PHOTOS
L102	PH1 & PH2 LANDSCAPE SCHEMATIC

Building Area Summary

	PHASE 1		PHASE 2	
	HEATED	GARAGE	HEATED	GARAGE
LEVEL P1	4,265 S.F.	14,784 S.F.	9,686 S.F.	18,039 S.F.
LEVEL P2	10,610 S.F.	11,634 S.F.	1,242 S.F.	18,394 S.F.
PODIUM RESIDENTIAL	12,155 S.F.		18,052 S.F.	
LEVEL 2 RESIDENTIAL	12,240 S.F.		18,052 S.F.	
LEVEL 3 RESIDENTIAL	12,240 S.F.		18,052 S.F.	
LEVEL 4 RESIDENTIAL	9,192 S.F.		18,052 S.F.	
SUBTOTAL	69,702 S.F.	26,618 S.F.	83,136 S.F.	36,433 S.F.
COMBINED TOTAL	87,320 S.F.		119,569 S.F.	

Parking Phase 1

	QUANTITY	FACTOR	REQUIRED	PROPOSED
B.H.A. OFFICE	11,065 S.F.	1/500 S.F.	23	23
STUDIO	26 UNITS	1/UNIT	26	22
1-BEDROOM	30 UNITS	1/UNIT	30	26
2-BEDROOM	8 UNITS	1/UNIT	8	6
3-BED TOWNHOUSE	5 UNITS	1.5/UNIT	8	6
			95	83
	AFFORDABLE AND SENIOR HOUSING IS ALLOWED A PARKING REDUCTION VIA A PLANNING DIRECTOR WAIVER			

Residential Unit Summary

	PHASE 1	PHASE 2	TOTALS	
	BLDG. A	BLDG. B	BLDG. C	
STUDIO	26			26
1-BEDROOM	30	34	36	100
2-BEDROOM	8	4	7	19
3-BEDROOM			4	4
3-BED TOWNHOUSE	5			5
SUBTOTAL	69	38	47	154
COMBINED TOTAL	69		85	154

Parking Phase 2

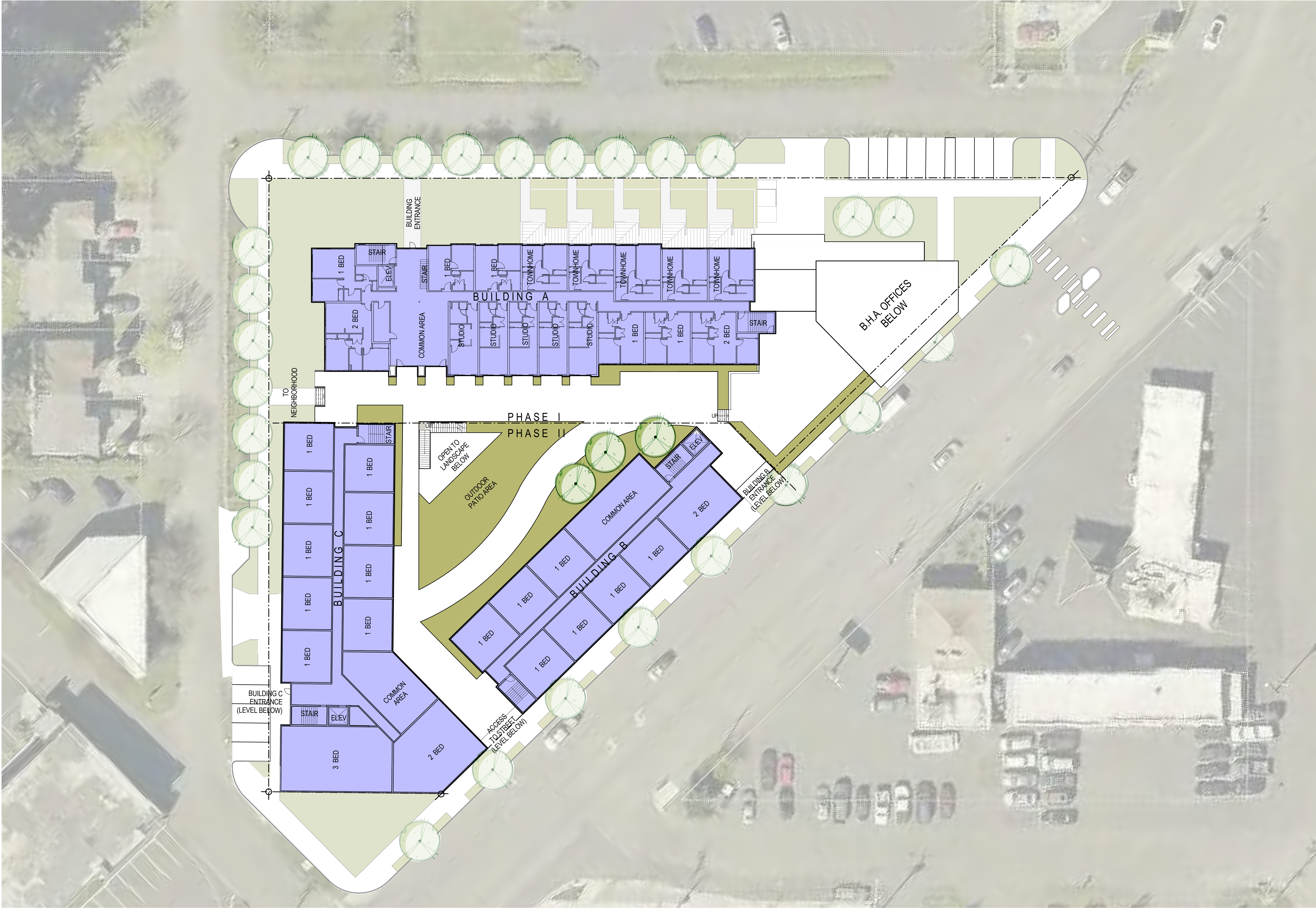
	QUANTITY	FACTOR	REQUIRED	PROPOSED
COMMON AREA	5,000 S.F.	N/A	N/A	N/A
COMMERCIAL / REST.	3,800 S.F.	1/500 S.F.	8	8
STUDIO	-	1/UNIT	-	-
1-BEDROOM	72 UNITS	1/UNIT	72	49
2-BEDROOM	12 UNITS	1/UNIT	12	9
3-BEDROOM	4 UNITS	1.5/UNIT	6	5
			98	71
	AFFORDABLE AND SENIOR HOUSING IS ALLOWED A PARKING REDUCTION VIA A PLANNING DIRECTOR WAIVER			



1 Street Level - P1
1" = 20'-0"



1 Upper Level - P2
1" = 20'-0"



1 Podium Level
1" = 20'-0"

Samish Way Redevelopment Partners LLLP
Samish Way Redevelopment (Aloha!) Master Plan
301 & 315 North Samish Way

Job No.: 1571.01 Date: 30 JAN 2019
File No.: 1571.01 BHA Aloha
Drawn By: CJ / RAW
Checked By: JMC
Issued for: Design Review

PODIUM LEVEL
PLAN

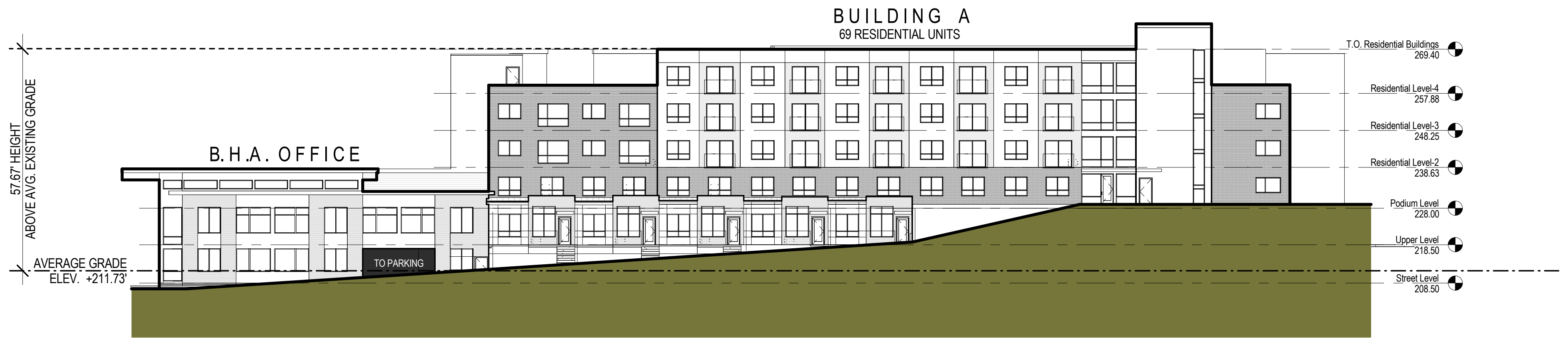
MP200



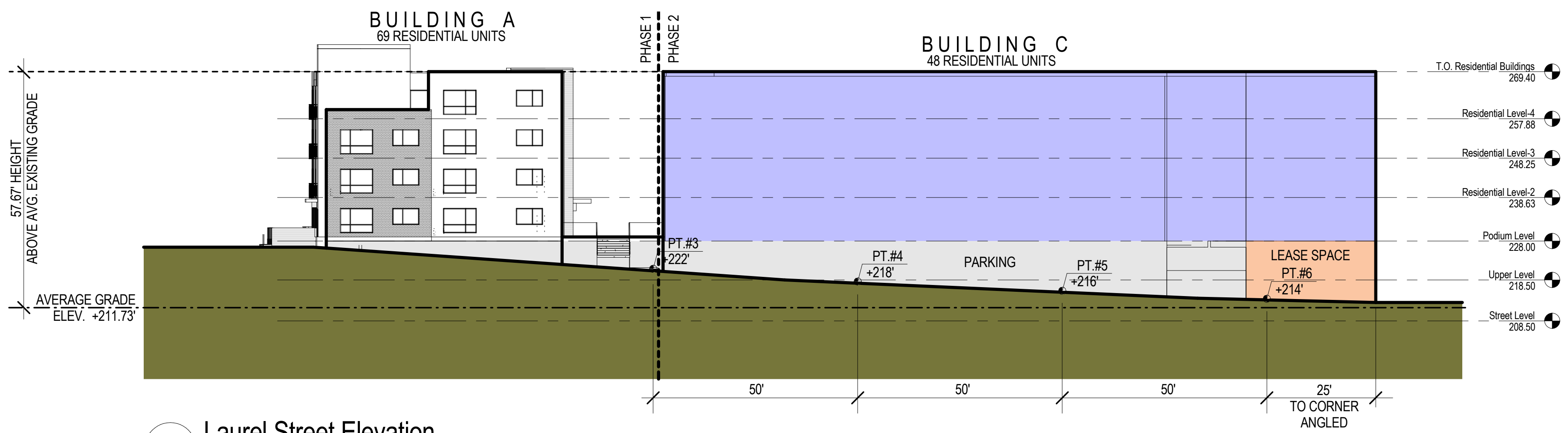
1 Typical Upper Levels 2 & 3 (4th Level Similar)
1" = 20'-0"



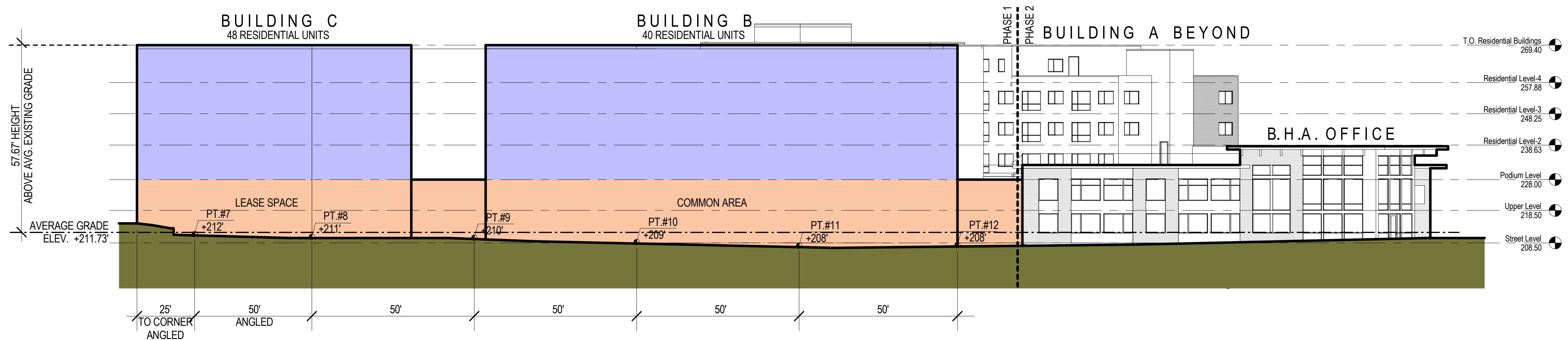
4 Aerial Perspective



3 Otis Street Elevation



2 Laurel Street Elevation



1 Samish Way Elevation



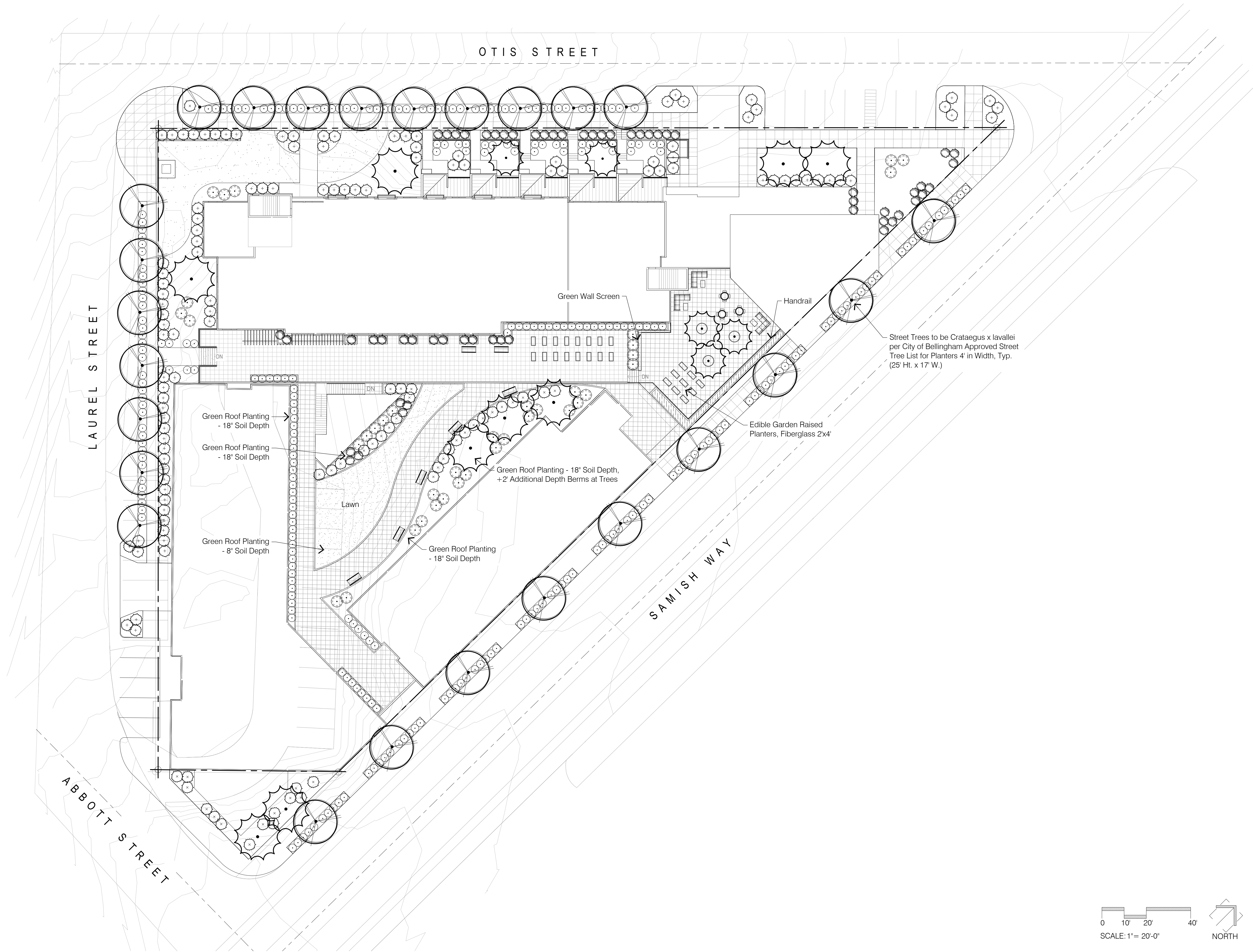
3 Abbott Street / E. Laurel Street
SCALE: NTS



2 Otis Street
SCALE: NTS



1 N. Samish Way
SCALE: NTS



Samish Way Redevelopment Partners LLLP
Samish Way Redevelopment (Aloha!) PHASE-I
315 North Samish Way

Job No: 1571.01 Date: 25 JAN 2019
File No:
Drawn By: PL
Checked By: PL
Issued for: Design Review

PH1 & PH2
LANDSCAPE
SCHEMATIC

L102

0 10' 20' 40'
SCALE: 1" = 20'-0"

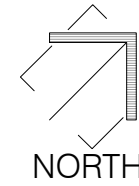




EXHIBIT E

Planning and Community Development Department

210 Lottie Street, Bellingham, WA 98225

Phone: (360) 778-8300 Fax: (360) 778-8301 TTY: 711 (WA Relay)

NOTICE OF INCOMPLETE APPLICATION & REQUEST FOR INFORMATION

Notice is hereby given that the Planning and Community Development Department (PCDD) has determined that this application is not sufficiently complete to begin review.

Date of Notice: June 24, 2019

Date Application Received: 5/28/2019 (Date permits paid for)

Project Location: 315 N. Samish Way; Sehome Neighborhood. Samish Way Urban Village – Commercial Transition 2 (CT2) zoning designation. Assessor's Parcel #: 380331 421256 and 425243

Applicant: Robert Wright, RMC Architects, 1223 Railroad Ave, Bellingham, WA 98225; Ph: (360) 676-7733

Property Owner: Bellingham Housing Authority; 208 Unity Street, Bellingham, WA 98225; Ph: (360) 676-6887

Application Type: Design Review (DR2019-0003), SEPA Checklist (SEP2019-0020), Parking Adjustment (ADM2019-0017)

The Planning and Community Development Department (PCDD) has reviewed the application(s) referenced above. It has been determined that these application(s) do not supply sufficient information to prepare a decision compliant with applicable regulations of the Bellingham Municipal Code (BMC) and Comprehensive Plan. To complete the application, please submit revised/additional information to comply with the following code requirement.

Required Actions:

To continue review of the above application(s), please submit the following information electronically to ckoch@cob.org and permits@cob.org, or in person to the permit center, located in City Hall:

- I. The parking reduction application does not provide sufficient information to prepare a decision in compliance with BMC 20.37.150(E), mainly that the information submitted does not indicate the extent that the, "uses (affordable housing) are inherently less auto dependent", or that the proposed mitigation, "will be adequate and proportionate to the requested parking reduction."
 - a. More information/analysis must be submitted such as from the ITE Parking Generation Manual to verify what reduction factor is typically associated with the proposed affordable housing and how much it may reduce the 72 parking stalls required by BMC 20.37.150(C)(1) for standard market-rate multifamily housing.

- b. Another option to consider is to request that the new street parking (at least 12 stalls) proposed to be developed by the project on Otis Street be counted as mitigation for the parking reduction on the basis that the street parking will increase shared public parking capacity in the vicinity which is substantially more efficient than private parking, and thereby decrease the need for inefficient on-site private parking. With the limited availability of on-site parking, the project will be less attractive to tenants who are auto dependent, and more attractive to tenants who are less auto dependent and more reliant on alternative transportation. The new improved street parking will not only be available to the tenants and the broader public in support of the urban village, it will also provide a physical barrier between the sidewalk and moving vehicular traffic which increases pedestrian comfort and safety, and makes walking more attractive, enjoyable, and likely to occur. Please justify how the mitigation of new improving on-street public parking is adequate and proportionate to the proposed 12 vehicle parking space reduction.

As additional mitigation, the 28 bicycle parking stalls being provided is 13 more than required.

- c. You might also consider using the shared parking provision in BMC 20.37.150(D) and the ITE Parking Generation Manual to show the offset between peak parking demand for the 23 parking stalls required for the office use and the peak parking demand for the 72 parking stalls required of the residential use.

II. The following comments are based on the projects compliance with the Urban Village Design Standards (UVDS) and Samish Way Urban Village design and development regulations:

- a. Marked up plans for Phase 1 are attached. Make changes as necessary and resubmit.
- b. Submit a letter from the USPS verifying the location of the mailboxes is acceptable.
- c. Submit a letter from Sanitary Serviced verifying the size and location of the garbage/recycling area is acceptable.
- d. The landscaping plan needs to show compliance with Green Factor landscaping pursuant to BMC 20.37.160(C). Include a utility conflict plan that is coordinate with the civil plans showing that tree plantings, particularly street trees within street rights-of-way, will not be located in close proximity to public utilities. Include details for root barriers where necessary. A Green Factor Worksheet is attached to assist you with the calculations.
- e. Revise your response to (B)(1)(b)(ii) and (iii)(A) in the project narrative to request a departure from locating parking at an intersection. The full site must meet applicable design standards since it is essentially being 100% redeveloped; existing conditions that are not in compliance must be brought into compliance or seek a minor modification. Each minor modification being requested must individually be justified against the minor modification criteria in BMC 20.25.070(A).
- f. Revise your response to (B)(2)(f)(ii) and (iii) in the project narrative to request a departure from minimum standards for awning depth (required to be greater than 4' when the awning will be more than 12' high) and awning wall coverage (75%

minimum). Note that the commercial frontage along Otis Street will be landscaped so an awning per the standards will be of no functional effect. Note that the awning will be at its deepest (6') where the main commercial entry is located. Overall, the awnings are designed to be essential elements to the building's architectural composition to establish an attractive anchor at this prominent intersection while providing weather protection where most necessary.

III. Regarding your May 22, 2019 letter requesting modifications to the street designs:

- a. The request in Exception No. 1 to lengthen the tree wells to 5' x 12' along Otis Street is acceptable given the project's residential frontage proposed along Otis Street and the added landscaping will improve the pedestrian realm/experience.
- b. See Jessica Bennett's comments below regarding Exceptions No. 2 and 3.

IV. The following comments are from staff with Fire, Public Works and Plans Examiners. Some of the comments are informational and will be addressed with the building permit and public facilities contract, but other comments may affect the site or building designs and require changes or additional information submitted to the design review permit application. For example, the street designs are a requirement of the development standards per BMC 20.37.140 (not design review), and therefore design changes will be a part of the public facilities review and approval. If you have specific questions about these comments, please contact the city staff person who made the comment.

- **Kim Barker, Plans Examiner.** The following comments are also shown on the attached set of plans.
 - 1) Accessible parking is required in both the surface lot and the underground parking. IBC 1106. Ensure lowest obstruction still permits van accessible height where applicable.
 - 2) Electric vehicle charging infrastructure is required for the B and R-2 parking. IBC 427
 - 3) Accessible dwelling units are required. IBC 1107
 - 4) Sanitary Service Co. (SSC) should approve the garbage enclosure location and it must have space for trash, recyclables and compost. WAC 51-50-009
 - 5) It appears this may not be applicable, but if 5 stories of Type VA construction are proposed, stair pressurization is required. IBC 504.4.1 Integrated early design with mechanical, electrical, fire protection disciplines are required. This would also require standby power in a dedicated equipment room.
 - 6) Two exits may be required from electrical room. IBC 1006 and NEC
 - 7) Units labeled as "townhouses" are not townhouses by IBC definition.
 - 8) Where vehicle barriers are provided in parking garage, structural design for a concentrated load of 6,000 lbs. 1607.8.3
 - 9) The City did not adopt the Washington Cities Electrical Code. The 2017 NEC is applicable per WAC.

- 10) Unenclosed exit access stairway must be demonstrated to comply with IBC 1019.
- 11) Confirm Roof Plan A206 complies as penthouse, or show compliance with story/height limitations. Specifically, IBC use limitations for penthouses. 1510.2.3
- 12) Commercial provisions of energy code apply. Dedicated outdoor air supply required in offices. Vestibules appear to be required at building entries, including BHA tenant area.
- 13) Elevator E1 to be sized to accommodate elevator gurney.
- 14) The method of code compliance regarding the 3-hour separation per 510.2 at the "townhouse" units are unclear. The 3-hour horizontal separation should be continuous from building exterior wall to exterior wall, but appears to be interrupted by the "townhouse" units. This may result in significant design changes.

- **Jessica Bennett, Public Works (Engineering)**

Public Works has not reviewed proposed frontage improvements beyond the general standard being provided. This review will happen with the PFC.

- 1) BMC 20.37.140 B requires improving Otis and Laurel to 3/4 street standard of the street design noted in the Samish Way Subarea plan, on Samish Way the abutting sidewalk must be improved to accommodate the street design in the Samish Way subarea plan, and Abbot must have a minimum 5' sidewalk provided. As part of the 3/4 street standard, street pavement must be brought up to standard. The existing condition of both roads is poor (pavement condition index of Otis 32 and Laurel is 36) and requires full reconstruction. All right-of-way improvements and public utility improvements (for example if a water main extension is required for an on-site hydrant) will be reviewed and approved through the Public Facilities Construction Agreement process. This should be applied for early in the process, but no later than concurrently with the building permit.
- 2) It is noted that a request for a modification of the street standard was submitted, however the full review of this can only occur with the PFC. At this completeness phase the submittal can be considered complete from a PW-Engineering perspective and additional comments were provided at recent internal meeting on the requested modification (Concern for safe backing up movements near the intersection of Samish particularly). It should also be noted that the current site plan will require additional right-of-way to be deeded. Where trees are planted a minimum 5x5 tree well must be provided.
- 3) Please ensure all existing public utility easements are showing on site plan as no structural improvements may encroach. Currently a stormwater vault, retaining wall, and generator appears to encroach into the easement along the right-of-way. A Phase 2 building appears to conflict with a utility easement from a previous vacation. The plan should be revised or the applicant must request for the easement to be relinquished. If that is desired it should be applied for early in the process as a public hearing is required.

- 4) Modification to an existing driveway is proposed off Abbott/ Laurel. Please note that this area must meet BMC 13.52.080 Corner clearance requirements. It appears what is proposed doesn't work, i.e. moves the driveway too close to the intersection.
- 5) Driveway from Otis must be a depressed curb return driveway as commented previously. Please review Public Works Development Guidelines for thresholds.

- **Jessica Bennett, Public Works (Stormwater)**

- 1) ~~No preliminary stormwater site plan provided. The project will exceed 5,000 square feet and must address minimum requirements no. 1-9. The plan should address at least a preliminary plan on how the requirements will be addressed, including if the site is a redevelopment or not per BMC 15.42 definition. The project will need to include frontage improvements, including reconstructing the roadways (see engineering comments). There was proposed storm facilities on the plans but it's unclear if/how the project will work for BMC 15.42. A storm vault appears to be proposed under a future building which is challenging from a building code stand point. How this vault will be maintained and how it will meet the intent of the Ecology Stormwater Manual standards is required.~~ **Provided 6/20/2019**
- 2) Required frontage improvements are considered part of the project and count towards stormwater mitigation thresholds. If stormwater thresholds are met or exceeded by the project, then there are three options for mitigating stormwater for off-site improvements. One option would be to provide stormwater mitigation off-site in a separate stormwater facility and dedicate the facility to the City. Another option would be to route the off-site stormwater on-site, and provide mitigation in a combined on-site, off-site stormwater system. This single facility is owned and maintained by the property owner. If a combined stormwater facility is desired, then a drainage release for the City is required. The drainage release is to waive the operation and maintenance requirements of the City on a privately owned stormwater facility, even though it mitigates stormwater from City right-of-way. The development section of public works can provide the instructions necessary to complete the drainage release. The third option is to over-detain the private property generated stormwater, in accordance with the current edition of the Stormwater Management Manual for Western Washington, in order to meet the flow duration standards for the project. Water quality treatment must be for the pollution generating impervious surfaces that are being created/replaced unless a swap of similar pollution generating characteristics is approved in the right-of-way.

- **Ron Richard, Fire Marshall:** No issues identified.

Review of the application cannot commence until all of the above referenced information has been submitted. Within 14 days after submittal of the requested information, staff will determine the completeness of the information and shall notify the applicant whether the application is complete or specify what additional information is necessary. **If all the requested information is not submitted within 120 days of the date of this notice (October 22, 2019), the application shall become null and void, in accordance with BMC 21.10.190(c). This is your only notice regarding the incomplete status of your application and no further notice will be sent concerning the expiration of the 120-day timeline.**

Please contact the staff member listed below if you have any questions.

Name: Chris Koch, Planner

E-mail / Phone: ckoch@cob.org or 360-778-8349



Attn:	Christopher J. Koch	
Company:	City of Bellingham	Date: 7/16/2019
Project:	Samish Way Redevelopment	<input type="checkbox"/> Transmittal
Job#:	1810	<input checked="" type="checkbox"/> Memo
From:	Robert Wright, AIA, LEED AP	<input type="checkbox"/> Phone Record
RE:	Notice of Incomplete Application & Request for Information	<input type="checkbox"/> Other: _____

Message

Chris,

Please find the response to your Notice of Incomplete Application & Request for Information. All responses to queries will be denoted in **blue**.

- I. The parking reduction application does not provide sufficient information to prepare a decision in compliance with BMC 20.37.150(E), mainly that the information submitted does not indicate the extent that the, "uses (affordable housing) are inherently less auto dependent", or that the proposed mitigation, "will be adequate and proportionate to the requested parking reduction."
 - a. More information/analysis must be submitted such as from the ITE Parking Generation Manual to verify what reduction factor is typically associated with the proposed affordable housing and how much it may reduce the 72 parking stalls required by BMC 20.37.150(C)(1) for standard market-rate multifamily housing.
 - b. Another option to consider is to request that the new street parking (at least 12 stalls) proposed to be developed by the project on Otis Street be counted as mitigation for the parking reduction on the basis that the street parking will increase shared public parking capacity in the vicinity which is substantially more efficient than private parking, and thereby decrease the need for inefficient on-site private parking. With the limited availability of on-site parking, the project will be less attractive to tenants who are auto dependent, and more attractive to tenants who are less auto dependent and more reliant on alternative transportation. The new improved street parking will not only be available to the tenants and the broader public in support of the urban village, it will also provide a physical barrier between the sidewalk and moving vehicular traffic which increases pedestrian comfort and safety, and makes walking more attractive, enjoyable, and likely to occur. Please justify how the mitigation of new improving on-street public parking is adequate and proportionate to the proposed 12 vehicle parking space reduction.

As additional mitigation, the 28 bicycle parking stalls being provided is 13 more than required.
 - c. You might also consider using the shared parking provision in BMC 20.37.150(D) and the ITE Parking Generation Manual to show the offset between peak parking demand for the 23 parking stalls required for the office use and the peak parking demand for the 72 parking stalls required of the residential use.

BMC 20.37.150(E) does not require a demonstration that affordable housing is less auto dependent, but it qualifies it as inherently less auto dependent without need for justification. It



Message (continued)

lists affordable housing as a mitigating program that can be utilized to achieve the parking reduction. The section of code offers up to a 30% reduction in parking, and we are merely asking for 24%. Although not required by code, our original parking adjustment application offers additional mitigating factors to merit this reduction.

- II. The following comments are based on the projects compliance with the Urban Village Design Standards (UVDS) and Samish Way Urban Village design and development regulations:

- a. Marked up plans for Phase 1 are attached. Make changes as necessary and resubmit.

See attached updated plans as requested. Please note the following comments on mark-ups:

A200 - CPTED RECOMMENDATION: The restroom entry should not be in a hidden location down a hallway. It should be located internal to the office space to provide natural surveillance (visibility), especially given the hallway access to the garage.

Restroom are typically located in inconspicuous locations providing discrete access to users. The hallway in question is located in a secure hallway requiring key fobs to enter from the parking garage. Additionally, security cameras will be present in the hallway to monitor the entry point.

A302 - CPTED: Will this stair be a secure entry? Being open steps with a perch, it will attractive for loitering. Measures to deter trespass/loitering should be incorporated such as gated at street level, video surveillance, posted signage for no trespass, etc.

This stair is no longer necessary and has been deleted.

C105 – The driveway must be 50 feet back from the arterial intersection. It appears you are moving it closer to that intersection. This may not work.

The driveway is located greater than 50' away from Samish Way, see dimension on revised A101 sheet.

C105 – Phase 2 building to be located over storm vault.

The storm vault is an interim measure and will be removed during phase 2 construction. Phase 2 will include the installation of a permanent vault in a different location.

L102 – CPTED: Add hostile (prickly, etc.) landscaping at base of windows to provide privacy separation from the open space, and to deter trespass, loitering and vandalism.

Landscaping including prickly plants will be included in the area as requested.

L102 – What is this?

That is a PSE owned pad mounted transformer serving the proposed building.

L102 – Show ground cover between building and street.

This area between the landscaping at the building and the temporary gravel path has been shown as hydroseeded grass. Phase 2 will include street and sidewalk improvements in this area.



Message (continued)

- b. Submit a letter from the USPS verifying the location of the mailboxes is acceptable.

See attached signed memo. USPS has requested the placement of a loading/unloading space directly in front of the residential entry.

- c. Submit a letter from Sanitary Serviced verifying the size and location of the garbage/recycling area is acceptable.

See attached letter from Rodd Pemble.

- d. The landscaping plan needs to show compliance with Green Factor landscaping pursuant to BMC 20.37.160(C). Include a utility conflict plan that is coordinate with the civil plans showing that tree plantings, particularly street trees within street rights-of-way, will not be located in close proximity to public utilities. Include details for root barriers where necessary. A Green Factor Worksheet is attached to assist you with the calculations.

Please see attached landscape plans issued in the modified drawings set. Sheet L106 includes the green factor calculation. Sheet L105 includes COB ST-184 detail including root barriers. Included civil drawings locate street tree wells and utility connection locations.

- e. Revise your response to (B)(1)(b)(ii) and (iii)(A) in the project narrative to request a departure from locating parking at an intersection. The full site must meet applicable design standards since it is essentially being 100% redeveloped; existing conditions that are not in compliance must be brought into compliance or seek a minor modification. Each minor modification being requested must individually be justified against the minor modification criteria in BMC 20.25.070(A).

The DRB approval we are seeking constitutes well less than 100% redevelopment, as it specifically covers Phase 1 of the project. Please see response in attached revision to the project narrative.

- f. Revise your response to (B)(2)(f)(ii) and (iii) in the project narrative to request a departure from minimum standards for awning depth (required to be greater than 4' when the awning will be more than 12' high) and awning wall coverage (75% minimum). Note that the commercial frontage along Otis Street will be landscaped so an awning per the standards will be of no functional effect. Note that the awning will be at its deepest (6') where the main commercial entry is located. Overall, the awnings are designed to be essential elements to the building's architectural composition to establish an attractive anchor at this prominent intersection while providing weather protection where most necessary.

Please see response in attached revision to the project narrative.

III. Regarding your May 22, 2019 letter requesting modifications to the street designs:

- a. The request in Exception No. 1 to lengthen the tree wells to 5' x 12' along Otis Street is acceptable given the project's residential frontage proposed along Otis Street and the added landscaping will improve the pedestrian realm/experience.

Thank you.

- b. See Jessica Bennett's comments below regarding Exceptions No. 2 and 3.



Message (continued)

IV. The following comments are from staff with Fire, Public Works and Plans Examiners. Some of the comments are informational and will be addressed with the building permit and public facilities contract, but other comments may affect the site or building designs and require changes or additional information submitted to the design review permit application. For example, the street designs are a requirement of the development standards per BMC 20.37.140 (not design review), and therefore design changes will be a part of the public facilities review and approval. If you have specific questions about these comments, please contact the city staff person who made the comment.

- **Kim Barker, Plans Examiner.** The following comments are also shown on the attached set of plans.

- 1) Accessible parking is required in both the surface lot and the underground parking. IBC 1106. Ensure lowest obstruction still permits van accessible height where applicable.

OK. An accessible parking space will be provided in the temporary parking lot and a van accessible parking space will be provided with 98" vertical clearance.

- 2) Electric vehicle charging infrastructure is required for the B and R-2 parking. IBC 427

Electric vehicle charging stations will be provided on both P1 & P2 parking levels.

- 3) Accessible dwelling units are required. IBC 1107

The proposed development includes four barrier free units and two hearing and vision impaired units. They are located throughout the building and represent different sized units.

- 4) Sanitary Service Co. (SSC) should approve the garbage enclosure location and it must have space for trash, recyclables and compost. WAC 51-50-009.

See attached approval letter from Rodd Pemble at SSC.

- 5) It appears this may not be applicable, but if 5 stories of Type VA construction are proposed, stair pressurization is required. IBC 504.4.1 Integrated early design with mechanical, electrical, fire protection disciplines are required. This would also require standby power in a dedicated equipment room.

This development is 4 stories of Type VA construction.

- 6) Two exits may be required from electrical room. IBC 1006 and NEC

Only 1 exit is required from the electrical room per IBC 1006 & NEC.

- 7) Units labeled as "townhouses" are not townhouses by IBC definition.

OK.

- 8) Where vehicle barriers are provided in parking garage, structural design for a concentrated load of 6,000 lbs. 1607.8.3

OK, building permit submission will include structural drawings delineating loads.

- 9) The City did not adopt the Washington Cities Electrical Code. The 2017 NEC is applicable per WAC.



Message (continued)

OK, this code reference will be updated.

- 10) Unenclosed exit access stairway must be demonstrated to comply with IBC 1019.

This stairway has been deleted. This egress traffic will exit through stairwell access on the BHA rooftop.

- 11) Confirm Roof Plan A206 complies as penthouse, or show compliance with story/height limitations. Specifically, IBC use limitations for penthouses. 1510.2.3

Penthouse complies per 1510.2.3 – Uses include stair shaft, elevator shaft, vertical shaft, elevator control room and mechanical room.

- 12) Commercial provisions of energy code apply. Dedicated outdoor air supply required in offices. Vestibules appear to be required at building entries, including BHA tenant area.

OK, building will comply with WSEC. Vestibules will be provided at residential building entrances. The BHA tenant area does not require a vestibule per WSEC C402.5.7 exception 8, as its SF is less than 10,000 sq. ft. and it less than 4 stories above grade.

- 13) Elevator E1 to be sized to accommodate elevator gurney.

Yes, the elevator has been sized to accommodate an elevator gurney.

- 14) The method of code compliance regarding the 3-hour separation per 510.2 at the "townhouse" units are unclear. The 3-hour horizontal separation should be continuous from building exterior wall to exterior wall, but appears to be interrupted by the "townhouse" units. This may result in significant design changes.

This will be clarified in permit submission, but the 3 hour separation will exist and extend from the podium level up and over top of the "townhouses."

- **Jessica Bennett, Public Works (Engineering)**

Public Works has not reviewed proposed frontage improvements beyond the general standard being provided. This review will happen with the PFC.

- 1) BMC 20.37.140 B requires improving Otis and Laurel to 3/4 street standard of the street design noted in the Samish Way Subarea plan, on Samish Way the abutting sidewalk must be improved to accommodate the street design in the Samish Way subarea plan, and Abbot must have a minimum 5' sidewalk provided. As part of the 3/4 street standard, street pavement must be brought up to standard. The existing condition of both roads is poor (pavement condition index of Otis 32 and Laurel is 36) and requires full reconstruction. All right-of-way improvements and public utility improvements (for example if a water main extension is required for an on-site hydrant) will be reviewed and approved through the Public Facilities Construction Agreement process. This should be applied for early in the process, but no later than concurrently with the building permit.

The PFC contract will be submitted in short order, prior to the building permit submission. As a reminder, the city has agreed to allow BHA to defer improvements on Laurel until the second phase of construction.



Message (continued)

- 2) It is noted that a request for a modification of the street standard was submitted, however the full review of this can only occur with the PFC. At this completeness phase the submittal can be considered complete from a PW-Engineering perspective and additional comments were provided at recent internal meeting on the requested modification (Concern for safe backing up movements near the intersection of Samish particularly). It should also be noted that the current site plan will require additional right-of-way to be deeded. Where trees are planted a minimum 5x5 tree well must be provided.

We look forward to the PFC review for discussion of our intended plan and how that can work for the City and public.

- 3) Please ensure all existing public utility easements are showing on site plan as no structural improvements may encroach. Currently a stormwater vault, retaining wall, and generator appears to encroach into the easement along the right-of-way. A Phase 2 building appears to conflict with a utility easement from a previous vacation. The plan should be revised or the applicant must request for the easement to be relinquished. If that is desired it should be applied for early in the process as a public hearing is required.

We were unaware of the easement until alerted of this issue. We intend to apply for easement vacation in short order.

- 4) Modification to an existing driveway is proposed off Abbott/ Laurel. Please note that this area must meet BMC 13.52.080 Corner clearance requirements. It appears what is proposed doesn't work, i.e. moves the driveway too close to the intersection.

The existing driveway is a touch over 50' away from the Samish Way curb. We look forward to the PFC review for discussion of our intended plan and how that can work for the City and end user.

- 5) Driveway from Otis must be a depressed curb return driveway as commented previously. Please review Public Works Development Guidelines for thresholds.

We look forward to the PFC review for discussion of our intended plan and how that can work for the City and end user.

- **Jessica Bennett, Public Works (Stormwater)**

- 1) ~~No preliminary stormwater site plan provided. The project will exceed 5,000 square feet and must address minimum requirements no. 1-9. The plan should address at least a preliminary plan on how the requirements will be addressed, including if the site is a redevelopment or not per BMC 15.42 definition. The project will need to include frontage improvements, including reconstructing the roadways (see engineering comments). There was proposed storm facilities on the plans but it's unclear if/how the project will work for BMC 15.42. A storm vault appears to be proposed under a future building which is challenging from a building code stand point. How this vault will be maintained and how it will meet the intent of the Ecology Stormwater Manual standards is required. Provided 6/20/2019~~

Ok.

- 2) Required frontage improvements are considered part of the project and count towards stormwater mitigation thresholds. If stormwater thresholds are met or exceeded by the project, then there are



Message (continued)

three options for mitigating stormwater for off-site improvements. One option would be to provide stormwater mitigation off-site in a separate stormwater facility and dedicate the facility to the City. Another option would be to route the off-site stormwater on-site, and provide mitigation in a combined on-site, off-site stormwater system. This single facility is owned and maintained by the property owner. If a combined stormwater facility is desired, then a drainage release for the City is required. The drainage release is to waive the operation and maintenance requirements of the City on a privately owned stormwater facility, even though it mitigates stormwater from City right-of-way. The development section of public works can provide the instructions necessary to complete the drainage release. The third option is to over-detain the private property generated stormwater, in accordance with the current edition of the Stormwater Management Manual for Western Washington, in order to meet the flow duration standards for the project. Water quality treatment must be for the pollution generating impervious surfaces that are being created/replaced unless a swap of similar pollution generating characteristics is approved in the right-of-way.

The off-site stormwater will be routed on-site for treatment. Please see stormwater drainage report.

EXHIBIT F

Koch, Christopher J.

From: Justin West <cjustinwest@gmail.com>
Sent: Wednesday, July 31, 2019 11:15 AM
To: Koch, Christopher J.
Subject: re aloha project

Chris, I was reviewing plans for the aloha project and I had some questions. In particular regarding the parking plan. It looks as though the intention is to reduce the code prescribed number of parking spaces for this project due to it being "low income" We already have some parking issues in this area and I was hoping to have a discussion with you about this proposal that they are putting forward.

thanks

Justin

360 927 9255



Virus-free. www.avg.com

Koch, Christopher J.

From: susan haines <susan.haines@hotmail.com>
Sent: Monday, May 6, 2019 6:29 PM
To: Koch, Christopher J.
Subject: Samish Way Aloha Complex concerns

Dear Chris Koch,

I am writing to you because I am unable to attend the council meeting this evening due to a work commitment.

I was alerted to the changes in the scope of the Aloha apartment complex yesterday. I was shocked to discover that changes have been made so the plans include a height increase of 75 feet; quite a difference from the other structures on Samish and VERY out of place in the historic Sehome neighborhood and that the building will now provide 160 units of low income housing.

I live two blocks up from the old Aloha motel at 3304 Abbott St. I am a single mom with a 9 year old daughter and we were so happy when the Aloha closed and the specific "Aloha" populations that were hanging out on sidewalks, trails, stoops, any available space, leaving behind needles and drug paraphernalia, feces, and more in their wake, were no longer roaming around our house.

I finally felt safe in my home, and felt safe allowing my daughter to walk a few blocks to her friends' houses without these populations hanging around.

To be clear--I DO SUPPORT creating more affordable housing opportunities for Bellingham citizens. However, I am VERY concerned about the size of this complex.

While I support low income housing, the sheer enormity of the place does not seem to be a wise decision. Where will all of these people "hang out"? I am asking the city to reconsider the allowable size of the complex and change it to a smaller size that will not impact the area so much with an influx of residents that will change the neighborhood. My daughter and I finally feel safe in our home and neighborhood after the "Aloha Motel" years--please do not allow these areas to go back to this!

There is no infrastructure in place on Samish way to support the addition of 160 units, with 2-4 people (or more?) living in them. The traffic during the morning and evening "rush hour" from Samish to Bill McDonald is already quite congested--not to mention the traffic in the area from Sehome High school and WWU employees. I cannot imagine the issues of traffic once you add 160 more apartments there, in addition to the student housing going in beside Walgreens. The Samish Way corridor cannot handle this much traffic without affecting the safety of pedestrians and the residents of the Sehome neighborhood.

I am on faculty at WWU and experience the traffic issues on Samish multiple times a day. Two weeks ago as I was taking my daughter to school at Happy Valley Elementary, we were in a three car accident (the third car bumped) due to the already extreme traffic conditions on Bill McDonald Pkwy. It is unthinkable to add 300+ cars to this area--the stop and go traffic will extend through the I-5 ramps and to the stoplights onto Lakeway/Holly St. This much traffic will be a dangerous addition for the pedestrian traffic of students and drivers trying to get to school and work.

I hope the city will reduce the size of this complex in order to preserve the safety of pedestrians and drivers alike.

Thank you for your time, I hope a wise decision can be made that supports housing needs in Bellingham but does not destroy existing community regions that do not have the requisite infrastructure in place to handle the influx of people.

Sincerely,

Koch, Christopher J.

From: Eva Chazo <evachazo@liquidoak.com>
Sent: Wednesday, April 17, 2019 12:54 PM
To: Koch, Christopher J.
Cc: Jose Chazo
Subject: Aloha Site Development

Follow Up Flag: Follow up
Flag Status: Flagged

Hello Mr Koch,

Yesterday I spoke with Robert Wright an architect leading the Aloha site development. I live in the historic Sehome Hill neighborhood but missed the first two meetings regarding this development since meeting notification letters were only mailed out to people living within 500 feet and not all of Sehome Hill.

I have serious concerns about the City of Bellingham's decision to build a 160 units, twin tower complex that is 75 feet tall abutting a historic neighborhood where no buildings are to exceed 35 feet. The neighboring homes will not just be overshadowed, but will essentially be blocked by a structure of this size. This complex the city is planning will also more than double the population and traffic in this very small area of town. I am frankly astounded that such a plan is in place. This lacks true consideration of the Sehome community. I agree that more affordable housing certainly needs to be built and the Samish corridor is ripe for this; however, development needs to be planned with a conscience, not just simply to cram as much in as possible.

Please let me know the next meeting date or how I can find out when this meeting will be happening. I will be there.

My address is Eva Chazo, 817 Newell St, Bellingham, WA, 98225.

My phone is (360) 510-0023


I will be expecting a return email to confirm that I have been added to your mailing list.

Thanks,
Eva Chazo
Sent from my iPhone

Dear City of Bham.
& Council members
I am a Life Time Tax Payer
to own property on Sehome
Hill for 46 years & lots
of changes are here.
Samish Way is No Place
to raise a family.
about 300 more families
on a very busy old 99
& when the merging
of 5 Traffic Need to use
Samish & Police or fire
Truck Need there it will
be more clogged & very
congested NOT a fun Place
to live or play.

Thank you all -
David J. Welch
8-20-2019.

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Vote no on Samish Way housing development
3/18/19


~~James~~ David Welch

To the citizens of Bellingham...and not Bellevueham,

I was born and raised here.

The population sign at Lake Padden in the 50's read 25,000. I remember it well before the freeway came and bought our 3 acre farm for 5,000 dollars on 32nd st. Bellingham's population has quadrupled since then!


How many folks here was born in the area .

Yes, my wife and I have owned property on Newell st since 1974 and we raised our 2 boys the

So they want to build a 160 unit housing project on Samish Way. 50 units are maybe ok for this area. Right now it can be up to a five minute wait just to get onto Samish way at rush hour from Laurel or Newell st. This apartment complex will add at least a 100 more cars and increased transportation vehicles to the neighborhood making a congested area even more so. How many folks here would live in the 160 units??????????

2/

Vote no on Samish Way housing development
3/18/19


~~James~~ David Welch

Emergency vehicles and busses will have a more difficult time reaching their destinations.

Where will the children play who live in these boxes? There are already two marijuana shops along Samish way next to the proposed project. Low income housing historically has more crime and drug use. With its location we may end up with the next generation of drug users and criminals.

Lets think about what we want Bellingham to look like in the next 5 or 10 or 20 years. Where will our children find a safe place to play?

I am not in favor.

Don't "Ballard-ize" our beautiful Bellingham. Please preserve it for our next generation. Thank you. David J Welch and family..

Send written comments and requests for information to:Name: **Chris Koch, Planner** E-mail / Phone: **ckoch@cob.org** or **360-778-8349**

Planning and Community Development Department - City Hall

210 Lottie Street - Bellingham, WA 98225 Fax: 360-778-8302

** The City of Bellingham seeks to comply with the American Disabilities Act. If you have special needs, please call 360.778.8300 (voice) or 360.778.8382 (TTY).

315 N. Samish Way**PRE2019-0019**

If you want to receive notification of any future actions regarding this project and to be formally notified by mail, please complete and return this section to the Planning and Community Development Department, City Hall, 210 Lottie Street, Bellingham, WA 98225.

Attn: Chris Koch, Planner **Yes, I would like to know the action taken and to be placed on the formal mailed notice list.**

Name:

JEREMY HENDREN

Address:

(including City, Zip)

1313 E MAPLE ST #201BELLINGHAM, WA 98225

Email:

MANAGER@GATEWAYTOTALOFFICE.COM

* PLEASE, PLEASE, PLEASE MAKE SURE
PARKING IS AMPLE. STREET PARKING
IS VERY LIMITED IN OUR NEIGHBORHOOD.

THANK YOU,

JEREMY HENDREN



Exhibit G

Planning and Community Development Department

210 Lottie Street, Bellingham, WA 98225

Phone: (360) 778-8300 Fax: (360) 778-8301 TTY: (360) 778-8382

Email: planning@cob.org Web: www.cob.org

Determination of Non-Significance (DNS)

SEP2019-0020

Date of Issuance of Threshold Determination: 10/22/2019

Project Description: Phased redevelopment of the former Aloha Motel site located at 315 N. Samish Way. The full redevelopment of this site is proposed to include commercial uses along the entire Samish Way frontage, structured parking and approximately 160 affordable residential units. **Phase 1**, which is the subject of this determination, includes 69 apartments, street front office along Samish Way, two structured parking levels, and street improvements on Otis Street with public street parking. The total square footage of the proposed building is 87,320 square feet, and the site is 26,036 square feet. The project is located within the Samish Way Urban Village Design Review District and has received design review approval. The permit decision approves departures from design standards. It also approves a request to reduce the on-site parking requirement from 95 to 72 spaces for affordable housing and also in lieu of other mitigation such as improvement of public street parking (13 stalls), increased bike parking, shared parking and location on and near WTA transit routes, in accordance with the Samish Way Urban Village Plan.

Project Location: 315 N. Samish Way. Sehome Neighborhood, Area 15; Samish Way Urban Village – Commercial Transition 2 (CT2) zoning designation

Proponent: Bellingham Housing Authority, Attn: Ken Van Winkle; P.O. Box 9701, Bellingham, WA 98227-9701; Ph: (360) 676-6887

Lead Agency: City of Bellingham, Planning and Community Development Department (PCDD)

Environmental Information Considered: Environmental Checklist dated 6/21/2019; Combined Design Review and Parking Reduction Permit (DR2019-0003 & ADM2019-17) dated 10/22/2019, including associated Design Review Permit Application with preliminary site and building plans and Parking Reduction Application; Storm Water Management Report (6/14/2019); Samish Way Urban Village Subarea Plan (2009); Urban Village Design Standards in Bellingham Municipal Code (BMC) 20.25.070; Samish Way Urban Village Development Standards in BMC 20.37.100-.170; written public comment (X5); Staff site visits.

The lead agency has determined that the requirements for environmental analysis, protection, and mitigation measures have been adequately addressed in the development regulations and comprehensive plan adopted under chapter [36.70A](#) RCW, and in other applicable local, state, or federal laws or rules, as provided by RCW [43.21C.240](#) and WAC 197-11-158. Our agency will not require any additional mitigation measures under SEPA. This decision was made after review of the environmental information considered on file with the lead agency. This information is available to the public upon request.

- ☒ This DNS is issued after using the optional DNS process in WAC 197-11-355. A notice of Application and Optional DNS was issued on **8/7/2019**, there is no further comment

period on the DNS.

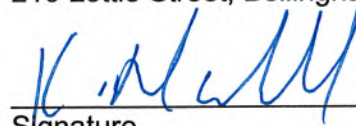
Appeal Rights: Pursuant to BMC 16.20.210(D), there is no administrative appeal of this environmental determination.

Staff Contact:

Chris Koch, Planner
ckoch@cob.org or 360-778-8349
Planning and Community Development Department
210 Lottie Street - Bellingham, WA 98225

Responsible Official:

Kurt Nabbefeld, Development Services Manager
Planning and Community Development Department
210 Lottie Street, Bellingham, WA 98225



Signature